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## SUMMARY

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# Wichita Bicycle Wayfinding Master Plan

## Steering Committee Meeting #2 September 9<sup>th</sup>, 2015, 4:30pm

### Attendees

Barry Carroll  
Jason Villanueva  
Eric Mead  
Chris Green  
Russell Fox  
Ikram Ahmed  
Larry Heotmer – COW, Parks  
Scott Wadle – Wichita-Sedgwick County Planning  
Paul Gunzelman – COW, Engineering  
Karen Vitkay – Alta  
Paul Wojciechowski - Alta

### Introductions

Scott Wadle welcomed everyone to the meeting and individuals introduced themselves to the group.

### Project Overview

Alta staff provided an overview of progress on the wayfinding system plan. The project team and City staff met with both KDOT and FHWA to discuss design requirements and funding implications. The project is on-time with the project schedule.

### Design Preferences

Karen Vitkay from Alta presented three wayfinding family design options-Keeper of the Plains, Wichita Flag, and Together Wichita. A voting exercise, a facilitated discussion was held. A public survey has been issued and will be open for comment until September 20<sup>th</sup>. Preliminary results were shared with the committee. Below are highlights from the discussion organized by the design options and overall comments.

#### Option 1 – Keeper of the Plains

- This execution of this concept is better than others.

- I like the bold contrast of the white silhouette on the dark background. Contrast is important to visibility.
- I like the curves.
- The kiosk is a better design than the other options.
- There may be confusion that the signs are directing people to the Keeper.
- I like the simple font.
- The Keeper is unique, there is nothing else like it in the nation. However it is ubiquitous, it is already used throughout the city.
- I like the maroon color but have concerns that the color might fade.
- I like the kiosk shape for the Keeper set

### Option 2 – Wichita Flag

- I like the idea of the flag, the graphic of the flag needs to be bolder on the off-street sign elements.
- I'm not sure the flag is a well-recognized symbol. It looks like it is from someplace else.
- The flag is growing in popularity, particularly among young people. It is a symbol of pride for the city.
- This is an emerging symbol for Wichita that isn't over used
- I like the flag as a symbol, but think the execution could be better.
- I like the colors shown in option 2 the best.
- I like the physical design
- I think the signs have too many sharp corners

### Option 3 – Wichita Together

- I like this design the best, it is big and bold. The over-sized signs are highly recognizable. The font of the logo reads well.
- This option is more neutral in terms of symbol.
- Continuing with this design would provide consistency in the system given that these signs have already been installed.
- I like the simplicity of the shape.
- I like that it represents more than Wichita
- I liked seeing them in person
- This option could provide opportunities to continue securing non-City of Wichita funding for wayfinding

### Preliminary Public Input –Wayfinding Online Survey #2

- Overall, option 1 with the Keeper currently has the greatest amount of public support.
- In terms of symbol alone, the option 1 Keeper of the Plains has received the most public support as shown with the current survey and previous surveys.
- Looking at color palette alone, the option 2 blue and red scheme is favored by a small margin.
- For overall shape, the option 3 trapezoidal shape is most favored by the community.

### Overall Comments

- I would be happy with any of the three designs.
- I was split between option 1 and option 2.
- Both the Keeper and Wichita Flag are Wichita symbols. We want to encourage more than Wichita. Should the design be transferable to other communities which may not identify with these items?

- The green and white are preferred colors for the on-street signs.
- The signs should be easy to read, no script lettering.
- Are there funding considerations between the options? Would the business group be interested in funding another option if a different design was selected?
- In the end the group was evenly split between options 1 and 2 (3 votes each) with the third option receiving one vote).

## Placement Recommendations

Karen Vitkay provided an overview of the wayfinding placement recommendations, including a PowerPoint presentation with slides showing graphics from the Design Options and Placement Practices document. Highlights from the discussion are provided below.

- The group had few comments on the placement recommendations.
- If anything, in plan view it appears that a lot of signs are being recommended.
- Clarification that confirmation could be achieved with either a sign or a pavement marking. Pavement markings are less obtrusive while still serving the intended function.
- Comment that multiple signs are needed, particularly on routes that are not linear.

## Project Prioritization and Readiness

Karen Vitkay provided an overview of the recommended prioritization model. The model was used to rank the bicycle facilities on multiple criteria presented in the Wayfinding Improvement Prioritization memo. The results of the model produced a heat map showing the existing and planned bicycle facilities prioritized for wayfinding improvements. For the most part, the group concurred with the assessment. Of the highly ranked routes, the committee thought that the Midtown Path, unless otherwise improved may not provide a positive cycling experience, and should not be included within the list of high priority projects. The committee members expressed that given the desire to provide a positive experience for cyclists, new facilities or those currently in design should be prioritized over older existing routes for wayfinding improvements.

The Steering Committee members were presented with a list of routes identified as highly suitable for wayfinding improvements and were asked to vote for the one they felt should be the top priority. The intent of the exercise was to identify a single project to serve as a pilot project – where conceptual plans for wayfinding improvements will be developed as part of the Wayfinding System Plan project.

The group identified the following three routes as most preferred for a pilot project (five votes each):

- 1<sup>st</sup> and 2<sup>nd</sup> Street bike lanes
- I-135 path
- Redbud path

The Arkansas River West and Zoo Boulevard Paths, also ranked highly receiving four votes each.

Following the Steering Committee's prioritization exercise and during the discussion, Karen Vitkay shared the preliminary current results from the citizen survey. The public voted most in favor of the 1<sup>st</sup> and 2<sup>nd</sup> Street bike lanes as the route most suitable for wayfinding improvements.

## **Next Steps**

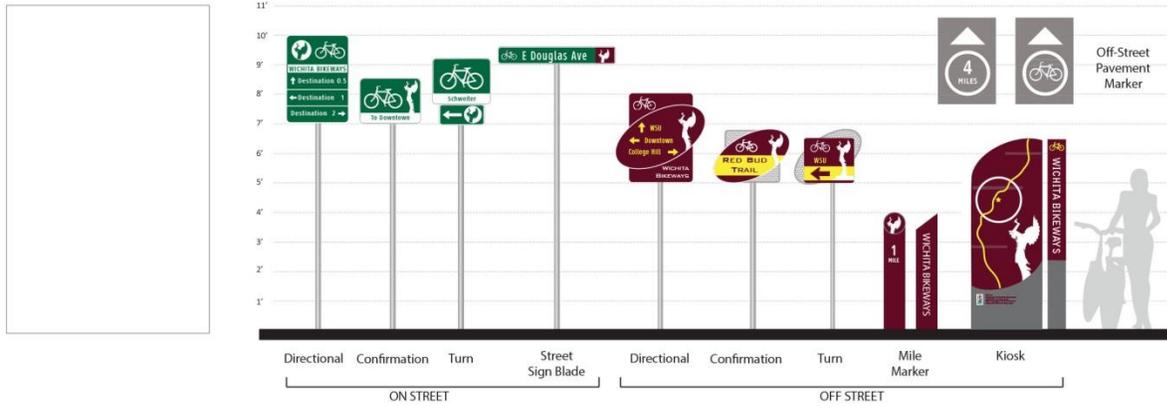
1. City to share the design options with Together Wichita and discuss their interest in funding future signs.
2. City to follow up with the family of the Keeper of the Plains artist regarding copyright and any limitations or requirements of using the Keeper as a symbol.
3. Public input survey to remain open until September 20<sup>th</sup>. Results will be summarized for consideration.
4. The designs and placement recommendations have been shared with KDOT for input. Awaiting input.
5. City to share design options and input with additional city staff and advisory boards, including the Design Council.
6. Consultant to update the designs once a preferred option and direction are provided by the City.
7. On September 15<sup>th</sup>, City Council shall consider the use of recently acquired funds for additional planning, design, and implementation. The additional work shall specifically address wayfinding in relation to health and equity.

# Design Options

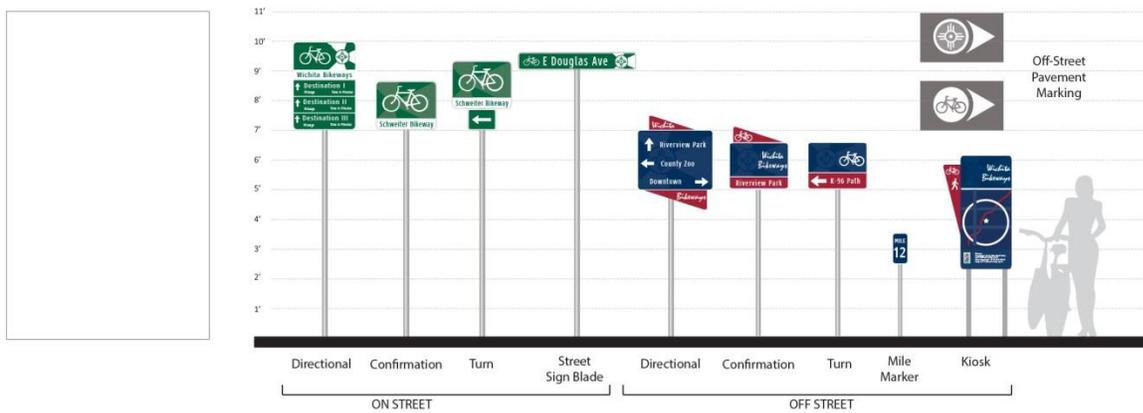
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## Option 1: Keeper of the Plains



## Option 2: Wichita Flag



## Option 3: Wichita Together

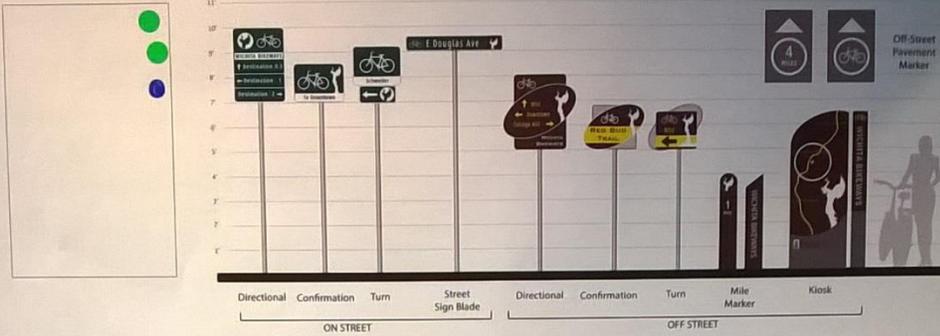


# Design Options

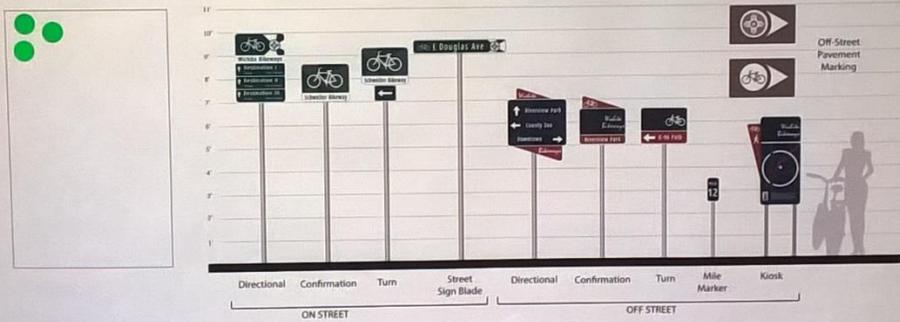
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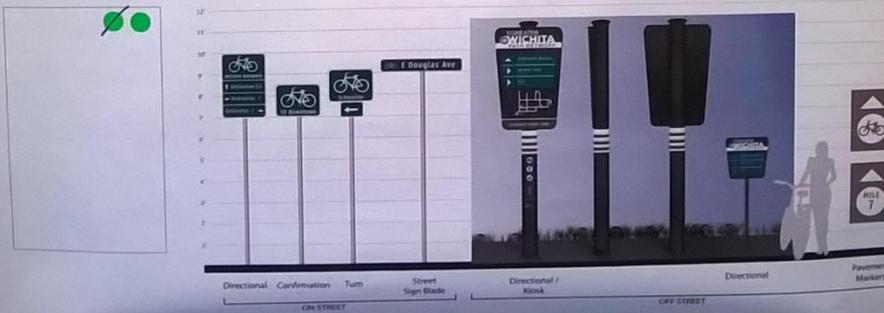
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## Option 2: Wichita Flag



## Option 3: Wichita Together



# Wayfinding Placement

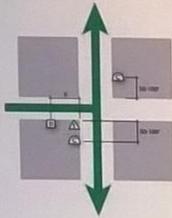
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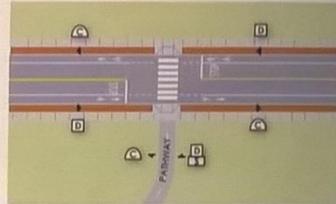
On-Street



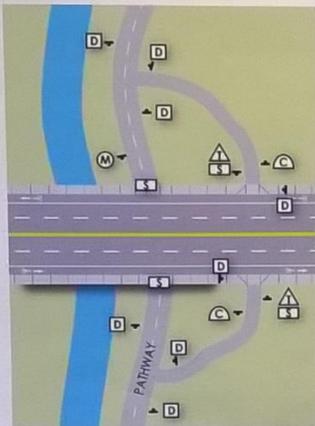
Off-Street



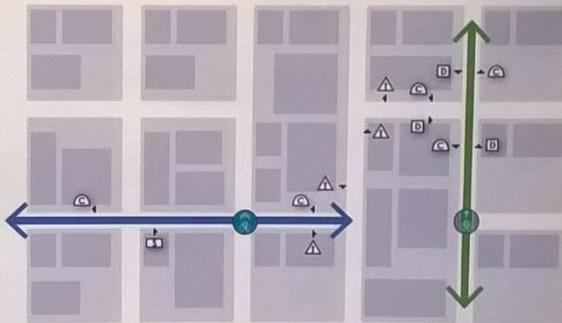
- Confirmation Sign
- Decision Sign
- Turn Sign
- Mile Marker
- Street Name



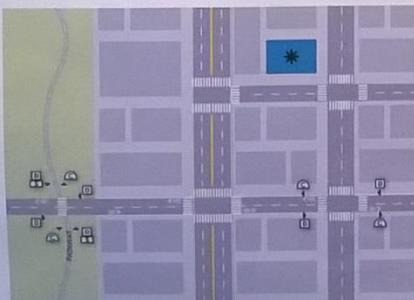
Facility Transition



Grade-separated Crossing



Facility Gap



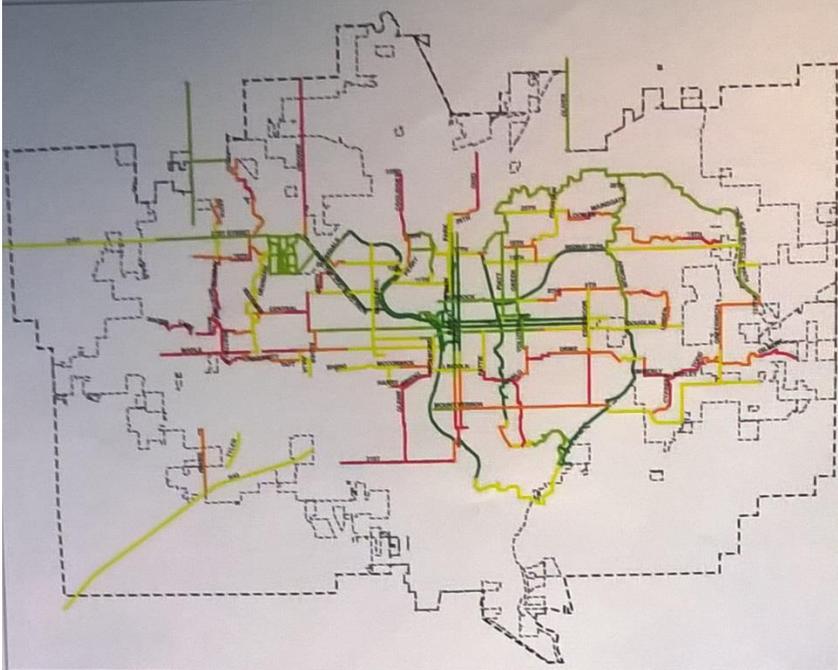
Off Network Destination



Off-Street/ On-Street Transition

# Route Suitability / Readiness

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## Network Wayfinding Prioritization

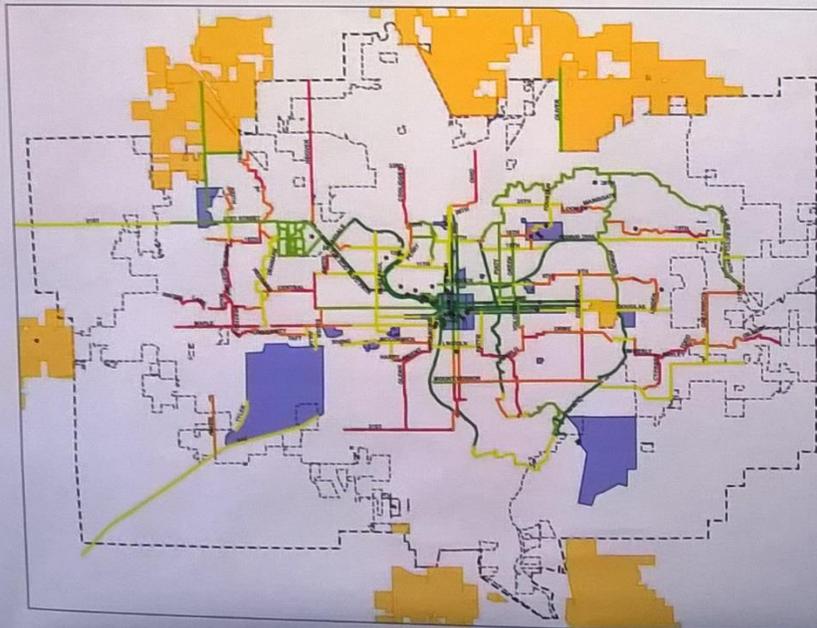
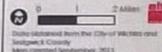
Wichita Bicycle Wayfinding

### LEGEND

#### Prioritization Ranking

- Lowest (0 - 4 total score)
- 5 - 8 total score
- 9 - 12 total score
- 13 - 16 total score
- Highest (17 - 21)

The prioritization ranking indicates the results of the prioritization scoring process based on three criteria - route readiness, proximity to destinations and landmarks, and bike network gap closure. These scores will be modified to incorporate the criteria of need/public input following feedback from City Staff, members of the Steering Committee, and the general public.



## Network Wayfinding Prioritization

Wichita Bicycle Wayfinding Plan

### LEGEND

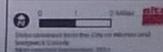
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#### Destinations

- Level 1 Destinations
- Level 2 Destinations
- Level 3 Destinations



# Route Suitability / Readiness

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## High Ranking Routes

Please select up to three routes for priority wayfinding improvements that would most improve cycling in Wichita.

- ● ● ● ● 1st and 2nd Street Bike Lanes (Seneca to Edgemoor)
- Arkansas River - East (approximately 13th St to Kellogg)
- ● ● ● ● Arkansas River - West (Westdale to Garvey Park)
- Keeper of the Plains
- ● ● ● ● I-135 Path (17th St to Pawnee)
- Gypsum Creek Path
- ● ● ● ● Redbud Path (I-135 to Woodlawn)
- Market and Topeka Bike Lanes (21st St to Kellogg)
- Midtown Path
- ● ● ● ● Zoo Boulevard Path
- Waterman (Arkansas River to Topeka)

*Empty Route?*





