

47th - 55th Street South Joint Area Plan

2008 - 2030

**47th to 55th Street South Joint Area Plan
2008-2030**

Prepared by:

The Plan Steering Committee &
Technical Advisory Committee
with assistance from:
The Wichita-Sedgwick County Metropolitan Area Planning Department

Under the Authority of:

Joint Sedgwick County / Wichita / Derby City Manager

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Executive Summary

Purpose of the Plan

In mid-2007, the City of Derby, the City of Wichita and Sedgwick County jointly agreed to develop a tri-jurisdictional land use plan for the unincorporated area located between 47th and 55th Street South, and K-15 to 159th Street East. It was determined that a plan with a 2030 planning horizon was needed to address issues related to the possible future extension of urban water and sewer services into portions of the plan area, and coordinate the long term 2030 urban growth areas for the cities of Derby and Wichita. The Wichita-Sedgwick County Metropolitan Area Planning Department offered its services in the development of this joint planning initiative.

A joint Plan Steering and Technical Advisory Committee was formed to advise and direct the development of this plan. The Committee also discussed the appropriateness of modifications to the 2030 growth areas for Derby and Wichita, and the current water agreement between Derby and Wichita.

The plan contains an 'existing conditions' analysis of natural features, land uses and services, zoning and land use policies affecting the plan area, as well as projections of future population and employment growth to the year 2030. The plan proposes an anticipated / desired future land use concept that reflects the efficient, future extension of various urban municipal services into portions of the plan area. It is anticipated that the plan for this area will be jointly adopted by Sedgwick County and the Cities of Derby and Wichita as elements of their respective comprehensive plans.

Summary of Outcomes

Future Land Use Concept

Based on research and Steering Committee and Technical Advisory Committee discussions, a Future Land Use Concept has been developed for the plan area that largely reflects the Future Land Use Map within the 2006 Derby Comprehensive Plan, and is consistent with the Wichita-Sedgwick County 2030 Functional Land Use Guide. The Future Land Use Concept developed for the plan area is a compilation of population and employment projections, existing and desired future land use patterns, future service area potential, implementation of a series of recommendations of the McConnell AFB Joint Land Use Study, and other current trends. It is intended that the Future Land Use Concept map will serve as an amendment to each jurisdictions' comprehensive plan.

Existing Water Agreement between Wichita and Derby

It is proposed that the existing water agreement between the City of Wichita and the City of Derby not be modified at this time, and the plan area remain within the City of Wichita's water servicing area. The City of Derby and the City of Wichita should continue to work together by allowing some flexibility in the agreement for situations/cases that arise in which the water agreement boundaries may need to be modified for economic and efficiency purposes. These situations should continue to be reviewed on a case-by-case basis. A map of Derby's contract service area may be viewed on Map #29.

2030 Urban Growth Area

The City of Derby 2006 Comprehensive Plan identifies a portion of the plan area (along Rock Road) as a part of the Derby 2030 Growth Area, while at the same time the Wichita-Sedgwick County Comprehensive Plan includes a large portion of the plan area within the City of Wichita's 2030 Urban Growth Area, from K-15 to just east of Greenwich Road. The 47th-55th Street South Joint Land Use Plan does not resolve future growth area/annexation issues. It is recommended that annexation continue to occur as the market demands.

Zoning Area of Influence

The City of Derby's Zoning Area of Influence currently covers the majority of the plan area from K-15 to 127th Street. The 47th-55th Street South Joint Land Use Plan does not propose changes to Derby's Zoning Area of Influence.

Phase I - Plan Framework

1.1 Plan Purpose

- To develop a tri-jurisdictional land use plan for Derby, Wichita, & Sedgwick County
- To determine the anticipated/desired future land use pattern that reflects the most efficient, future delivery of urban municipal services
- Serve as a basis for redefining the current 2030 urban growth areas for Derby and Wichita
- Lead to possible modifications to the current water agreement between Wichita and Derby regarding the provision and delivery of future water services in the plan area.

1.2 Plan Horizon

This Plan's recommendations are based on best available information and based on the anticipated urbanized growth within the plan area over the period of 2008-2030. Refer to the map on page 5 for the boundaries of the plan area.

1.3 Plan Development Process

Steering Committee

The role of the Steering Committee is to guide the development of the plan by utilizing information gathered by the Technical Advisory Committee. Steering Committee members consist of two Derby Planning Commissioners, two Wichita-Sedgwick County Metropolitan Area Planning Commissioners, Wichita City Councilmember (District II), a representative from the Derby City Council, and Sedgwick County Commissioner (District 5). (See acknowledgements -page 2)

Technical Advisory Committee

The role of the Technical Advisory Committee is to gather the best available information, analyze data, and provide recommendations to the Steering Committee so that informed decisions are made throughout the development of the plan. Technical Advisory Committee members consist of the MAPD Planning Director & Project Staff, Derby Planning Director, Derby City Manager, Wichita Assistant City Manager, Wichita Water Utilities, Wichita Public Works, Derby Community Development, Sedgwick County Public Works, McConnell AFB, Sedgwick County RWD #3, Westar Energy, Aquila. (See acknowledgements - page 2)

Planning Process To Date

Phase I—Plan Framework—April—May 2007

Phase II—Existing Conditions—June-August 2007

Phase III—Future Urban Growth Assumptions—August-November 2007

Phase IV—Future Municipal/Community Servicing Concept—August-November 2007

Phase V—Future Land Use Concept—October-February 2008

Phase VI—Tri-Jurisdictional Task Force—January 2008

Phase VII—Approval/Adoption of the Plan—March-April 2008

Planning Commission Participation

1st Meeting – Present a proposed “Future Municipal/Community Servicing Concept” and preliminary “Future Land Use Concept”

2nd Meeting – Present final recommended “Future Municipal/Community Servicing Concept” and “Future Land Use Concept”

Public Participation

1st Meeting – Overview of plan project/purpose/ timeline and display of “Existing Conditions,” proposed “Future Municipal/Community Servicing Concept,” and “Future Land Use Concept”

Phase II - Existing Conditions

2.1 Natural Features

Drainage Basins within the Plan Area

The East Basin of the Arkansas River drainage basin covers the vast majority of the 47th-55th Street Planning Area. The Planning Area contains four major conveyance systems including, Spring Creek, Dry Creek, the McConnell Slough and the Oaklawn Slough. The topography east of the Arkansas River exhibits more relief than west of the Arkansas River, with well defined drainage patterns at most locations, along with steep banks and narrow valleys. In addition, the Middle Walnut River Watershed lies along the eastern portion of the Planning Area, which is a legally organized watershed that levies taxes every other year. Below is a description of each conveyance system as identified in PEC's report.

Spring Creek

Spring Creek is the principal conveyance system collecting runoff within the East Basin. Spring Creek extends from Pawnee, between Webb Road and Greenwich Road, to a point near the intersection of 103rd Street South and Woodlawn, where it joins the Arkansas River. The Spring Creek basin has good relief and a well-defined main channel. The major tributaries to Spring Creek are Dry Creek and Trail Creek--all three of which pass through the City of Derby. As development occurs in this area, erosion of the channel banks along residential lots has become more pronounced in recent years.

Dry Creek

Dry Creek is the largest tributary to Spring Creek east of the Arkansas River. Dry Creek extends from 47th Street South near Rock Road, to a point near Kay Street in Derby, where it joins Spring Creek. The area that contributes drainage to Dry Creek is generally bounded by 47th Street to the north, 83rd Street to the south, Buckner Street to the west, and Rock Road to the east. A significant portion of the Dry Creek channel passes through the corporate limits of Derby, and impacts residential neighborhoods with both flooding and bank erosion problems.

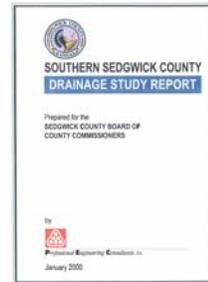
McConnell Slough

The McConnell Slough extends from the Air Force Base to the Arkansas River, near the intersection of 63rd Street South and Clifton Avenue. It collects runoff from a 8.5 square mile drainage area, bounded generally by 63rd Street to the south, Pawnee to the north, Webb Road to the east, and Hillside to the west. The main conveyance channel extends from the Arkansas River, crossing open span bridges at Oliver and Highway K-15, and around the east side of the Air Force Base Runway 1R/19L.

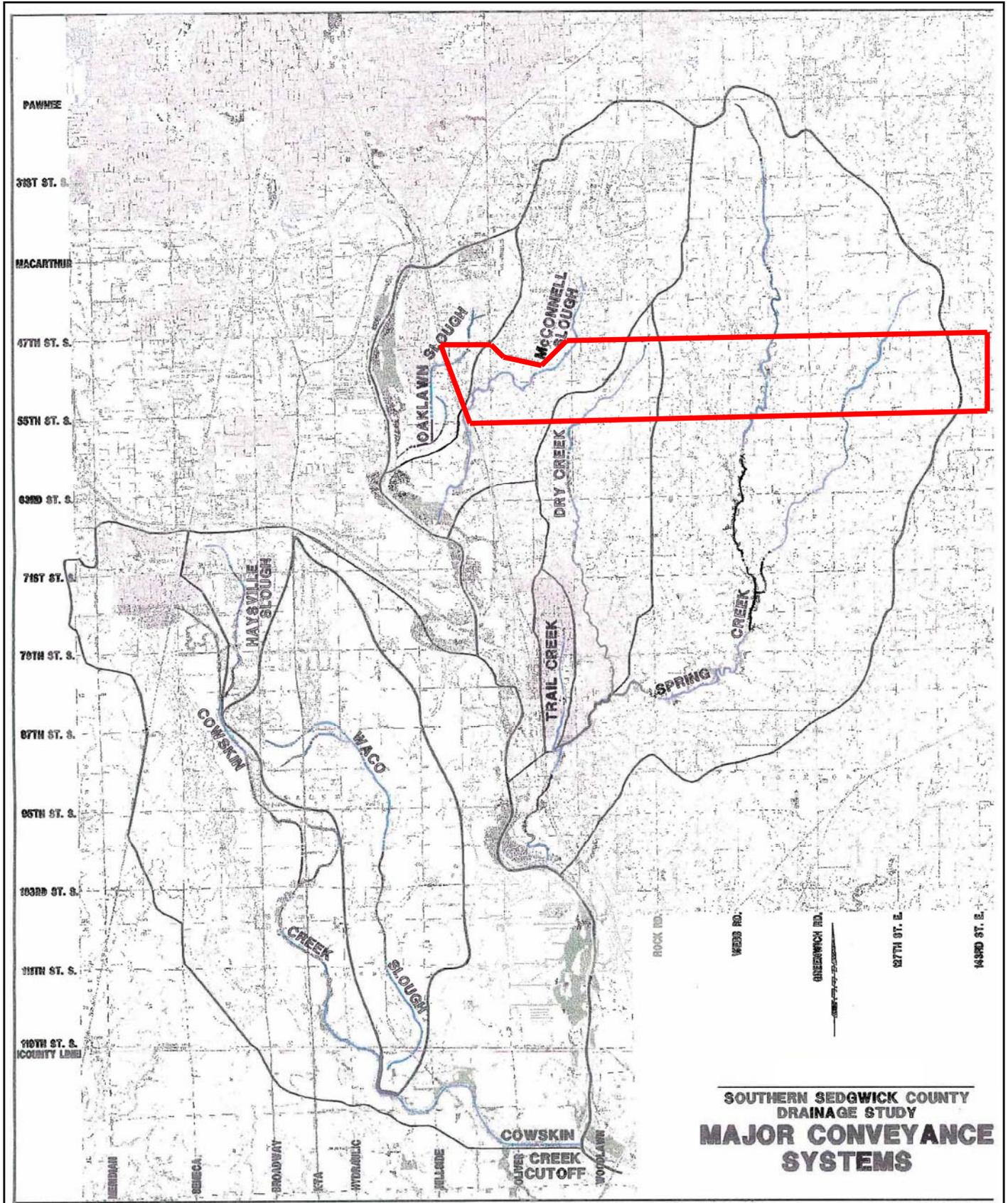
Oaklawn Slough

The Oaklawn Slough extends from one half mile north of 47th Street, near Oliver, to the Arkansas River, approximately one half mile south of 55th Street South. It collects runoff from a 2.0 square mile drainage area, bounded generally by 55th Street to the south, MacArthur Road to the north, Oliver to the east, and Clifton Avenue to the west. The main conveyance channel is well defined east of Clifton Avenue, between 55th Street and 47th Street. The Oaklawn Slough has good relief, with an elevation difference of 90 feet between the high and low ends of the basin.

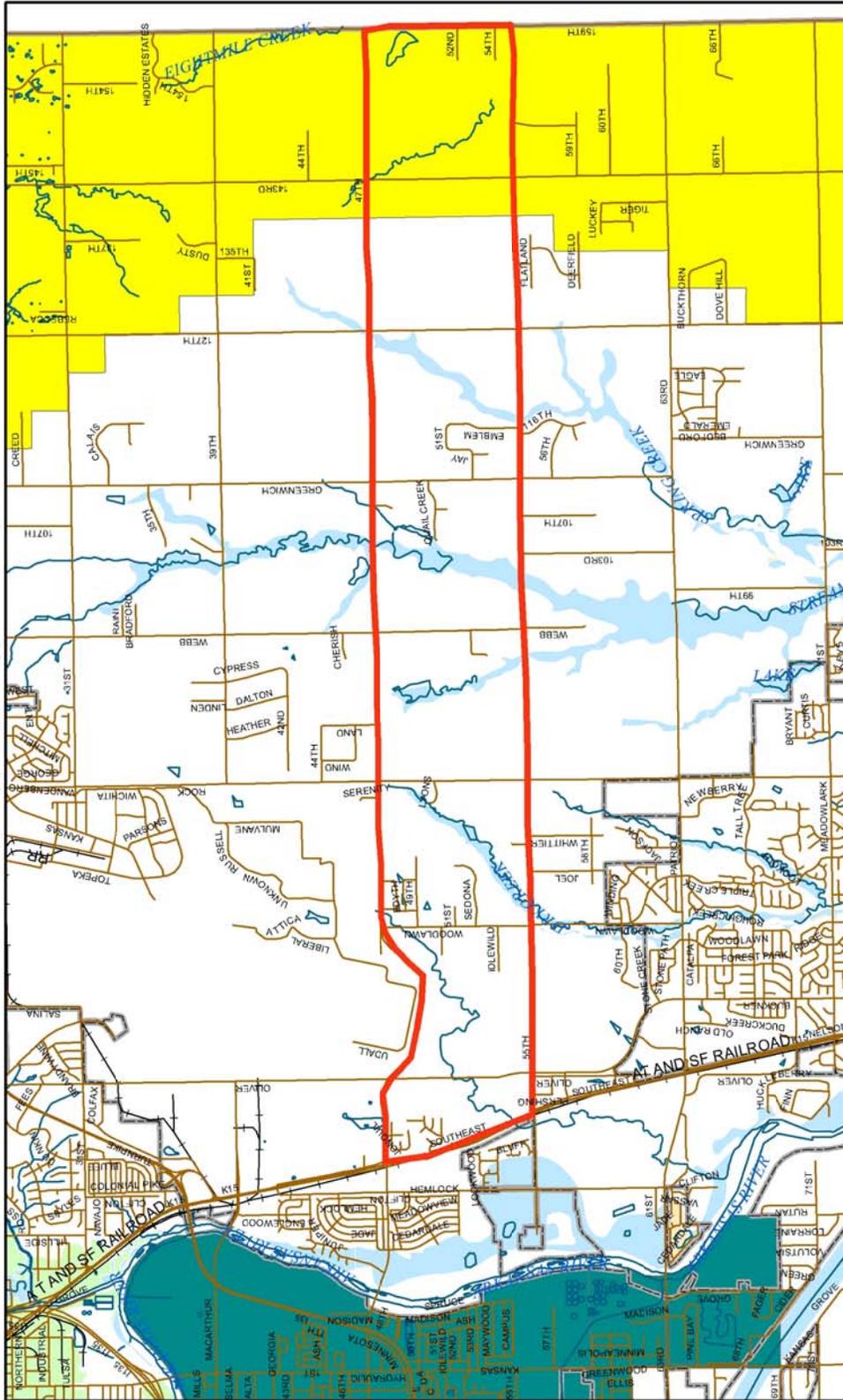
In January of 2000, Professional Engineering Consultants (PEC) prepared the "*Southern Sedgwick County Drainage Study Report*," for the Sedgwick County Board of County Commissioners to identify drainage problems that affect south central Sedgwick County and to provide a planning document for County Officials to use as a guide in the development of drainage improvement programs. Information throughout this section is from this study. PEC's study boundary was divided into two basins, east and west of the Arkansas River—the 47th-55th Street Planning Area being within the east basin.



Map 2: Major Conveyance Systems



47th-55th Street South Joint Area Plan Drainage Districts & Watershed Districts (Legally Organized)



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Software: ArcGIS
Map Data Sources:
City of Wichita
Sedgwick County
Prepared: 02/27/2007

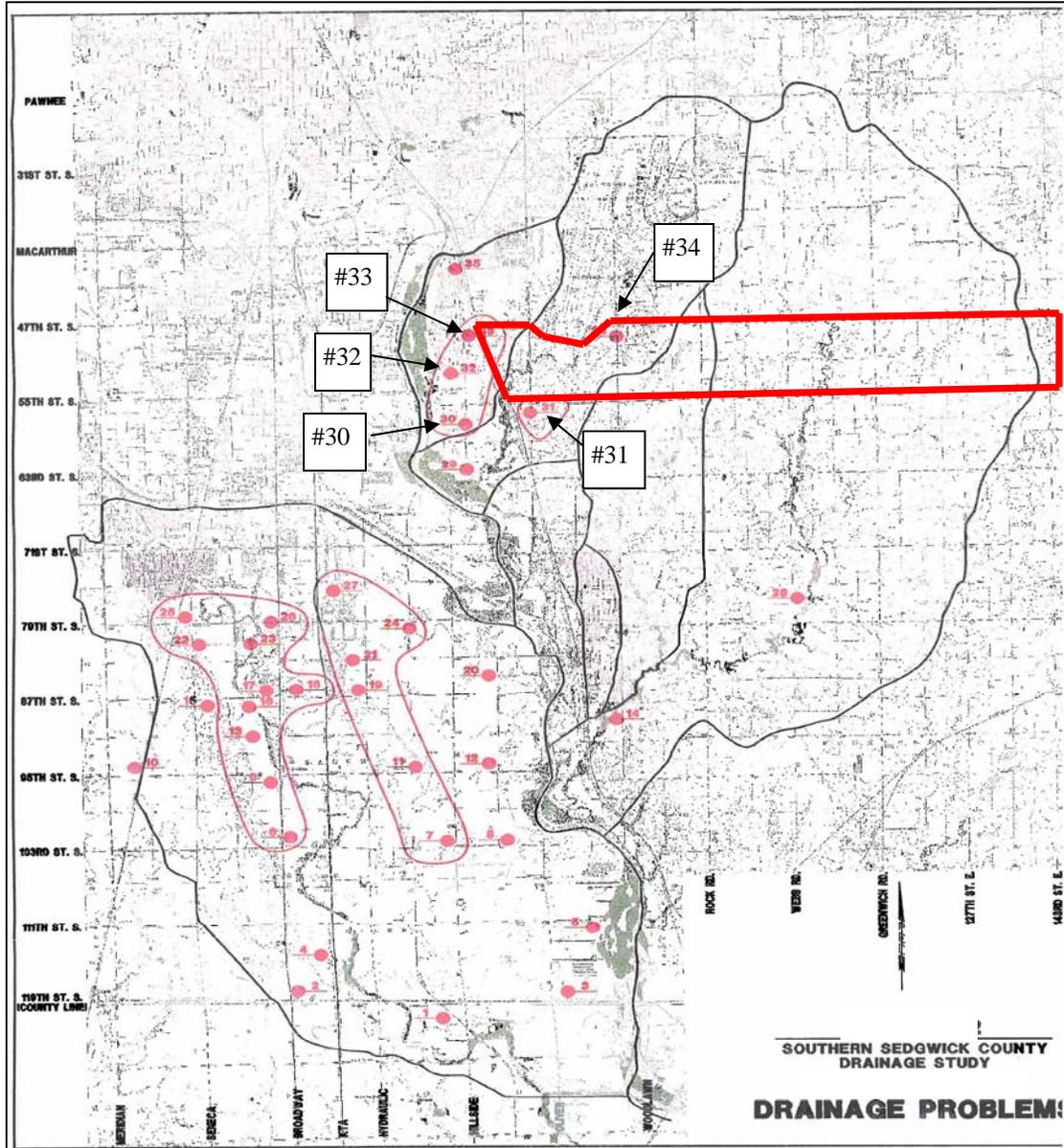


- Legend**
- Plan Boundary
 - Watershed
 - 0.2% Annual Chance Flood Hazard
 - A, AE, AH, AO
 - Middle Walnut River Watershed
 - X Protected by Levee
 - Riverside Drainage District
 -

Existing Drainage Problems

PEC's "Southern Sedgwick County Drainage Study Report" identified one low-priority drainage problem area (#34) within the 47th to 55th Street Planning Area. Several moderate-to-high priority problems are located outside the plan area to the west and southwest. Drainage problems #30, 31, 32 & 33 are scheduled improvements in 2008, according to the 2008-2012 Sedgwick County Capital Improvement Program.

Map 4: Existing Drainage Problems



Within Plan Area

Problem Area #34—Low Priority (Woodlawn, south of 47th St.)

Problem Description: The low water crossing on Woodlawn south of 47th Street is frequently overtopped by flood water. It was also noted that the stream channel is densely covered with trees and brush, which get washed downstream and tend to restrict or impede flow at bridge crossings.

Phase II—Existing Conditions: 2.1 Natural Features

Outside Plan Area—Moderate-to-High Priority

#32 - Moderate-to-High Priority (Idlewild Drive, west of Clifton Avenue in Oaklawn)

Problem Description: Idlewild Drive drains a large portion of the area within the Oaklawn Improvement District bounded by Clifton, 47th Street and Idlewild. Drainage is carried in the street gutters, but during heavy rains, stormwater is not contained within the street right-of-way (ROW). During heavy rains that fell in June 1997, several houses in the southern portion of Oaklawn were flooded. Water routinely stands at the street intersections after minor storms. Possible improvements may include a storm sewer system, and an improved outlet channel to the Arkansas River.

Problem Area #33—Moderate-to High Priority (South of 47th Street, between Clifton and K-15)

Problem Description: This is a low-lying area that is frequently covered with stormwater, which stands due primarily to lack of a well defined outlet. It is recommended that this area be considered in conjunction with other "High Priority" problems identified east of Clifton Avenue.

Outside Plan Area—High Priority

Problem Area #30—High Priority (Clifton St. from Idlewild to the bridge, one 1/4 mile south of 55th St.)

Problem Description: The east ditch along Clifton appears to be inadequate to carry the volume of stormwater that drains to this area. The ditch banks have been gradually eroding and this is of particular concern south of Idlewild, where the channel makes a turn to the south. During heavy rains, the flow in the channel attains sufficient velocity so that the momentum carries stormwater directly across Clifton. Over time, erosion of the west bank of the east ditch along Clifton will undermine the stability of the street. Clifton is also overtopped by stormwater south of 55th Street. The Sedgwick County Public Works Department has constructed channel improvements west of Clifton to the Arkansas River; however, the major problems noted are east of Clifton Avenue. The bridge crossing 1/4 mile south of 55th Street should be evaluated on a structural and a hydraulic capacity basis. The channel makes a hard right-turn just east of the bridge. The south bank is currently protected by riprap at the turn, but during high velocity flows in the channel, water continues to flow south.

Problem Area #31—High Priority (Residences between Oliver and K-15 south of 55th Street South)

Problem Description: Several houses west of Oliver are situated in a low area. The existing channel conveyance system that carries drainage from the northeast is inadequate. Stormwater flows across yards and near houses on a relatively frequent basis though this area.

Updated Flood Insurance Rate Maps

Wichita, Derby, and Sedgwick County are all participants of the National Flood Insurance Program and recently adopted revised/updated Flood Insurance Rate Maps effective February 2, 2007. These map changes brought approximately 1,400 Wichita properties into the 100-year floodplain for the first time and brought approximately 337 unincorporated properties into the 100-year floodplain for the first time. On the other hand, approximately 4,191 properties countywide were taken out of the 100-year floodplain.

Plan Area—Properties within the 100-Floodplain

Based on the new floodplain boundaries, it appears that the plan area contains approximately 64 parcels, in which the 100-year floodplain intersects. A total of 41 of the 64 are parcels with improvements. Only six (6) parcels have structures (homes or outbuildings) that are actually within the 100-year floodplain. So a total of approximately \$1,262,590 could potentially be at risk of damage as a result of flooding.

Public Entity Challenges

Many people mistakenly believe that each governmental jurisdiction is responsible for maintaining or improving the major drainage channels or creeks, but instead, each jurisdiction's role is limited to maintaining waterways that are within the public right-of-way, such as roadside ditches. The majority of streams are privately owned, and in the absence of easements, afford no right-of-way for public access or maintenance. Therefore, several areas that are not maintained by property owners have debris and/or are overgrown, thus increasing flooding risks.

Topography

The topography east of the Arkansas River exhibits more relief than west of the Arkansas River, with well defined drainage patterns at most locations. Overall, the plan area has a mostly uniform elevation. Variations ranging from 1300 feet to 1370 feet throughout the area, however the western part of the area contains the most significant slopes.

Prime Agricultural Land

According to the USDA Soil Conservation Service, "*Soil Survey of Sedgwick County, Kansas*," it appears that the majority (~75-85%) of the plan area is considered "prime agricultural land." Refer to Map #5.

Soil Permeability

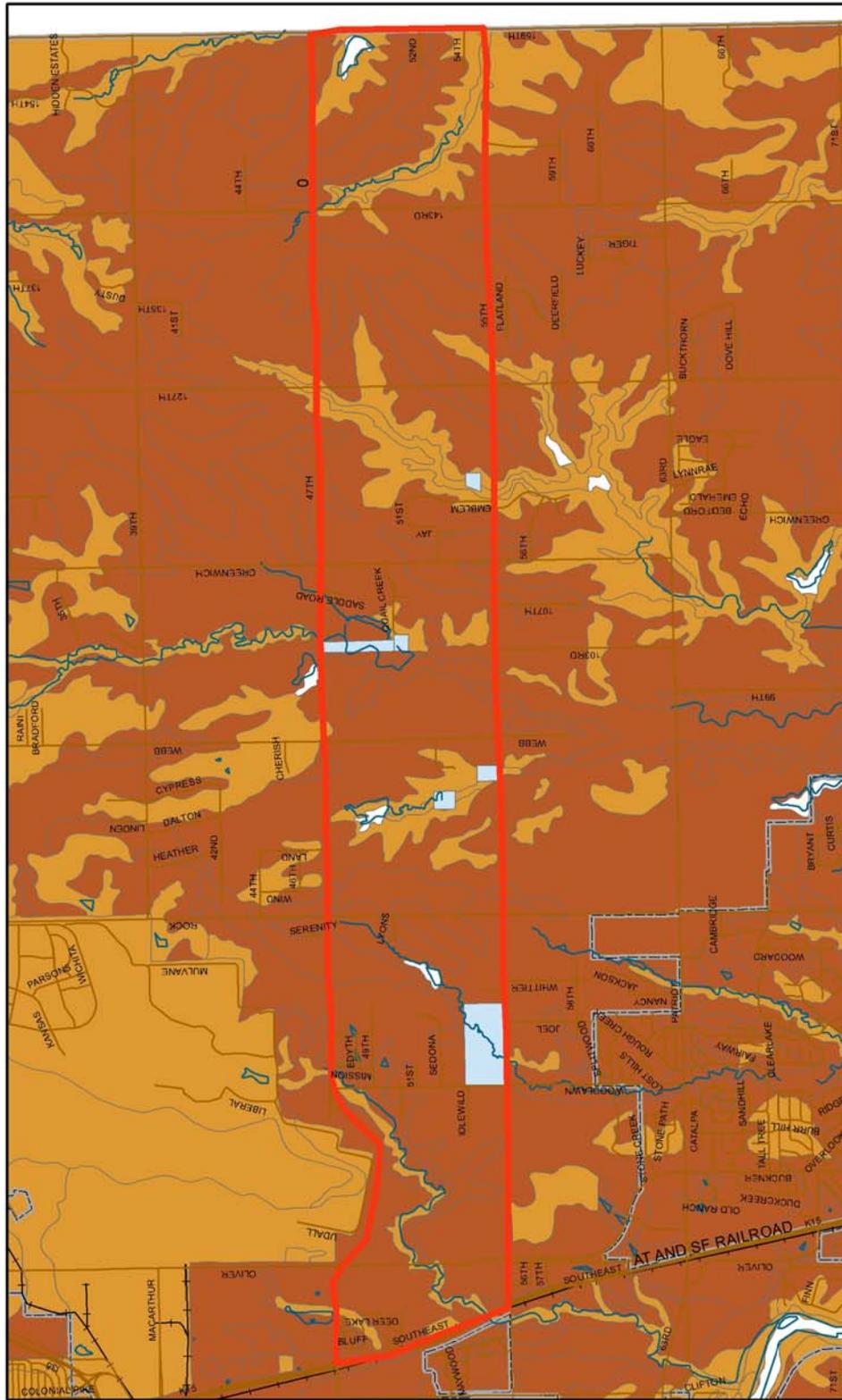
The type of soils in the plan area are important and directly impact the amount of runoff which will occur from any precipitation event. During the beginning stage of a typical precipitation event, soils may have a relatively high infiltrative capacity. As rainfall continues, or becomes more intense, the infiltrative capacity decreases, and consequently a larger portion of the rainfall becomes runoff.

National Flood Insurance Program

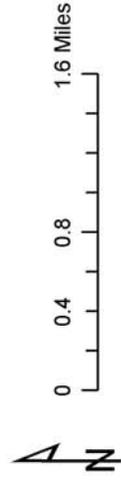
The U.S. Congress established the National Flood Insurance Program with the passage of the National Flood Insurance Act of 1968. The primary purpose of the legislation was to provide an insurance mechanism to evaluate the level of risk with regard to surface water flooding for building structures situated near identifiable flood prone areas. This information could then be used to establish actuarial rates for flood insurance policies. In addition to aiding in the determination of flood insurance risks and rates, the FEMA FIS is also used by participating communities to guide floodplain management decisions and policies. In order to maintain eligibility to participate in the NFIP, FEMA regulations stipulate that the community must maintain certain minimum standards in floodplain management.

Prime farmland is land that has the best combination of physical and chemical characteristics for producing food, feed, forage, fiber, and oilseed crops and is also used as cropland, pastureland, rangeland and/or forest. It has the soil quality, growing season and moisture supply needed to economically produce sustained high-yields of crops when treated and managed, including water management. In general, prime farmlands have an adequate and dependable water supply from precipitation or irrigation, a favorable temperature and growing season, acceptable acidity or alkalinity, acceptable salt and sodium content, and few or no rocks. They are permeable to water and air. Prime farmlands are not excessively erodible or saturated with water for a long period of time, and they either do not flood frequently or are protected from flooding.

47th-55th Street South Joint Area Plan Prime Farmland

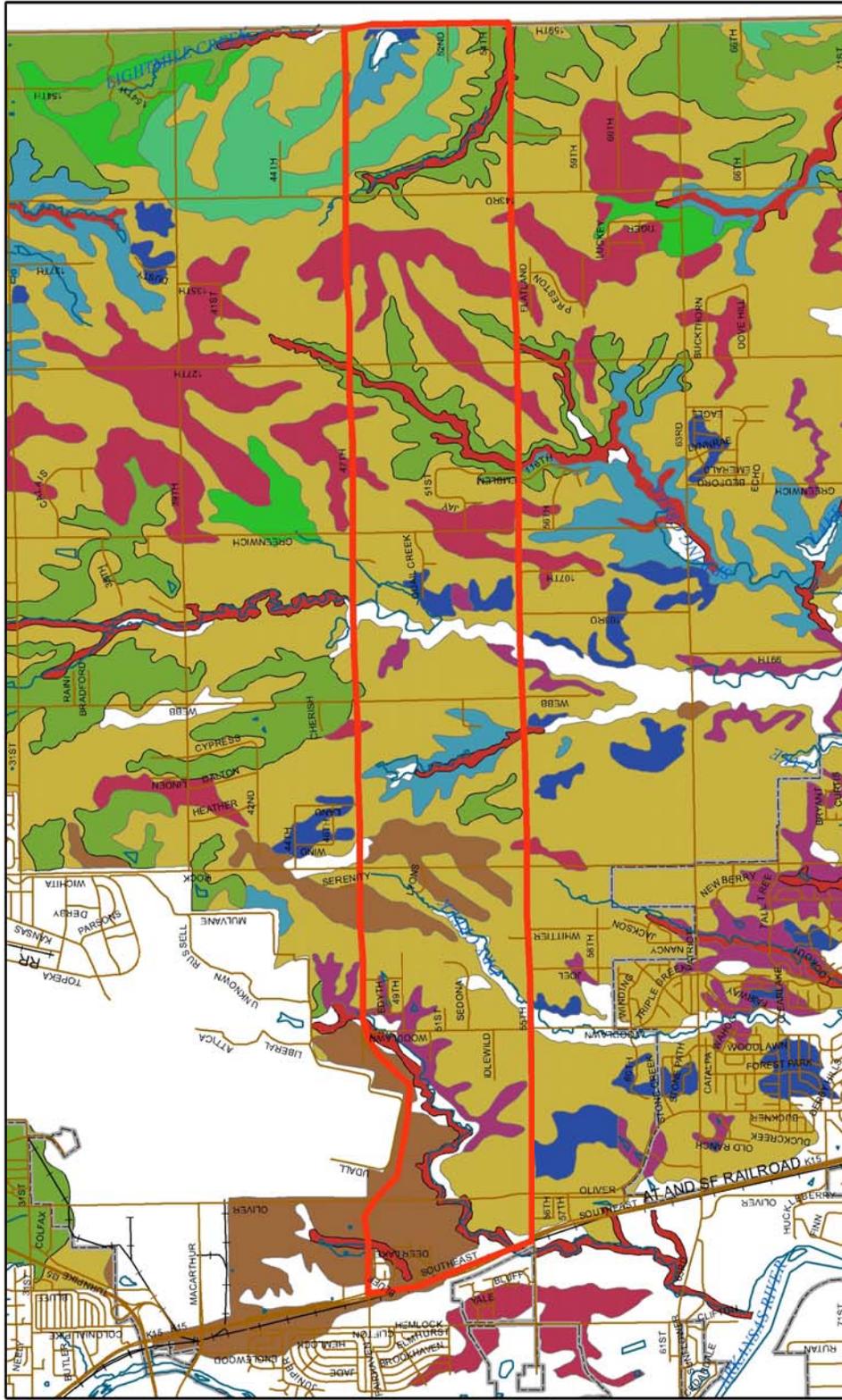


— Plan Boundary
Prime Farmland
 Not Prime Farmland (0)
 Prime Farmland (1)



Software: ArcGIS
 Map Data Sources:
 City of Wichita
 Sedgewick County
 Prepared: 8/13/2007
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47th-55th Street South Joint Area Plan Soil Classifications



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Software: ArcGIS
Map Data Sources:
Sedgewick County
Prepared: 8/27/2007

Plan Boundary

Soil Types

- Irwin silty clay loam, 1 to 3 percent slopes
- Irwin silty clay loam, 2 to 6 percent slopes, eroded
- Irwin silty clay loam, 3 to 6 percent slopes
- Ladysmith silty clay loam, 0 to 2 percent slopes
- Milan loam, 1 to 3 percent slopes
- Rosehill silty clay, 1 to 3 percent slopes
- Tabler silty clay loam

Soil Series

- Cline silty clay, 3 to 5 percent slopes
- Elmdoc silty loam, frequently flooded
- Elmdoc silty clay loam
- Goessel silty clay, 1 to 2 percent slopes

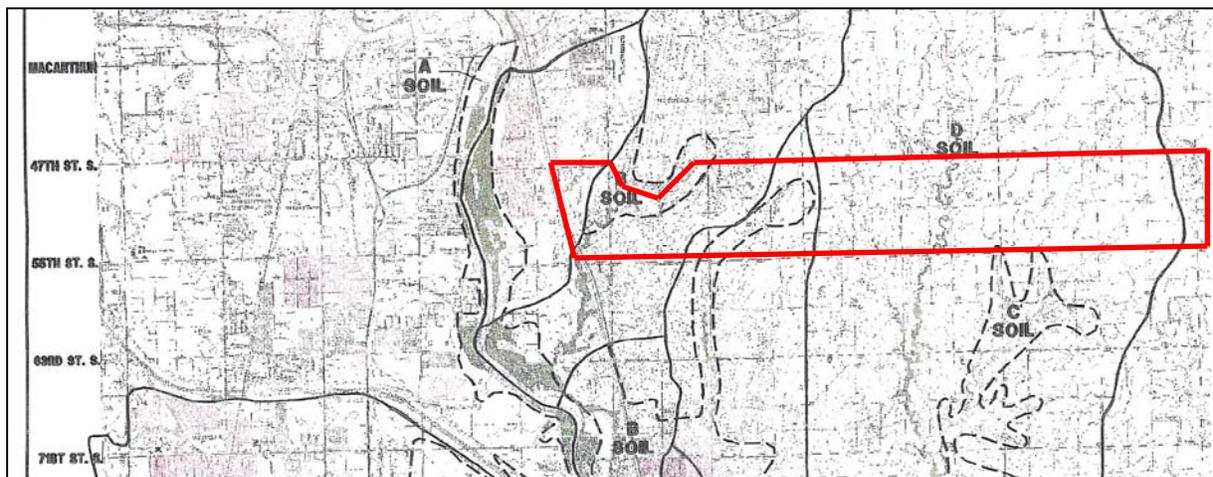
0 0.45 0.9 1.8 Miles

N

The Soil Conservation Service classifies soils into four hydrological groups, A through D, in order of decreasing permeability. The higher the permeability (Class A) the less runoff will occur and the lower the permeability (Class D) the higher the runoff. Other factors that affect the amount of runoff are vegetation cover, slope of the land, farming practices, and the antecedent moisture condition of the soil. The soils lying within the 47th to 55th Street Planning Area are predominately Class D soils--meaning that they tend to have low permeability and therefore high runoff rates.

Group D— Soils having a very slow infiltration rate (high runoff potential) when thoroughly wet. These consist chiefly of clay soils that have a high shrink-swell potential, a permanent high water table, a claypan or clay layer at or near the surface, and are shallow over nearly impervious material. These soils have a very slow rate of water transmission.

Map 7: Soil Hydrological Groups



Broad Land Use Considerations Based on Soil Types

In general, the soils that have good potential for cultivated crops also have good potential for urban development. Soil data can be helpful in planning future land-use patterns. Areas where the soils are so unfavorable that urban development is impossible are not extensive. Overall, the plan area primarily consists of Irwin, Rosehill and Tabler soils—all of which are better-suited for farming than for development. For urban development is costly on clayey soils due to the high shrink-swell potential. (See box below for further details.)

Irwin Silty Clay Loam Soil

A large portion of the plan area has Irwin Silty Clay Loam Soils (1 to 3 percent slopes), of which most areas of this soil type are farmed. The soil has fair potential for cultivated crops and good potential for hay, pasture and trees. Overall, it has poor potential for most engineering uses. Irwin Silty Clay soil has severe limitations for building sites due to the shrink-swell potential. The risk of structural damage could be reduced if basement walls, foundations, and footings are properly designed and reinforced and drain tiles are installed.

Rosehill Silty Clay Soil

Most areas of this soil is used for small grain and sorghum. The soil has fair potential for farming, trees and shrubs, and poor potential for most engineering uses. If buildings are constructed on this soil, foundations and footings should be designed to reduce the risk of structure damage caused by shrinking and swelling. The underlying shale beds are somewhat soft and are slip prone. Artificial drainage should be provided around the buildings to keep the soils and shale from becoming saturated.

Tabler Silty Clay Loam Soil

Most areas of this soil are farmed. The soil has fair potential for cultivated crops and good potential for hay, pasture, and trees. It has poor potential for most engineering uses. This soil is moderately well suited for corn, sorghum, and soybeans and is well suited for small grain, alfalfa, grasses for hay or pasture. Tabler Silty Clay Loam Soil has severe limitations for building sites due to the shrink-swell potential. The risk of structural damage could be reduced if basement walls, foundations, and footings are properly designed and reinforced and drain tiles are installed.

2.2 Other Features

Historic Sites

The Stearman Hangar and the Administration Building, both located within the McConnell Air Force Base, just 1/2 mile north of plan area, are listed on the National Register of Historic Places. Below are descriptions of each site. Refer to Map #8.

Stearman Hangar

The Stearman Hangar is located at what was originally the north end of the Old Municipal Airport, now the grounds of the McConnell Air Force Base. Stearman Aircraft Company built the structure in 1929 at a cost of \$42,000. The company used the one-story building for storage and display of new airplanes before they were delivered to a client. The south facade has eight cantilevered steel and glass sliding doors and is said to be the first aircraft hangar to utilize this door arrangement. The building has concrete footings, foundation walls and floors; the interior walls are thirteen inches thick and are made of brick.

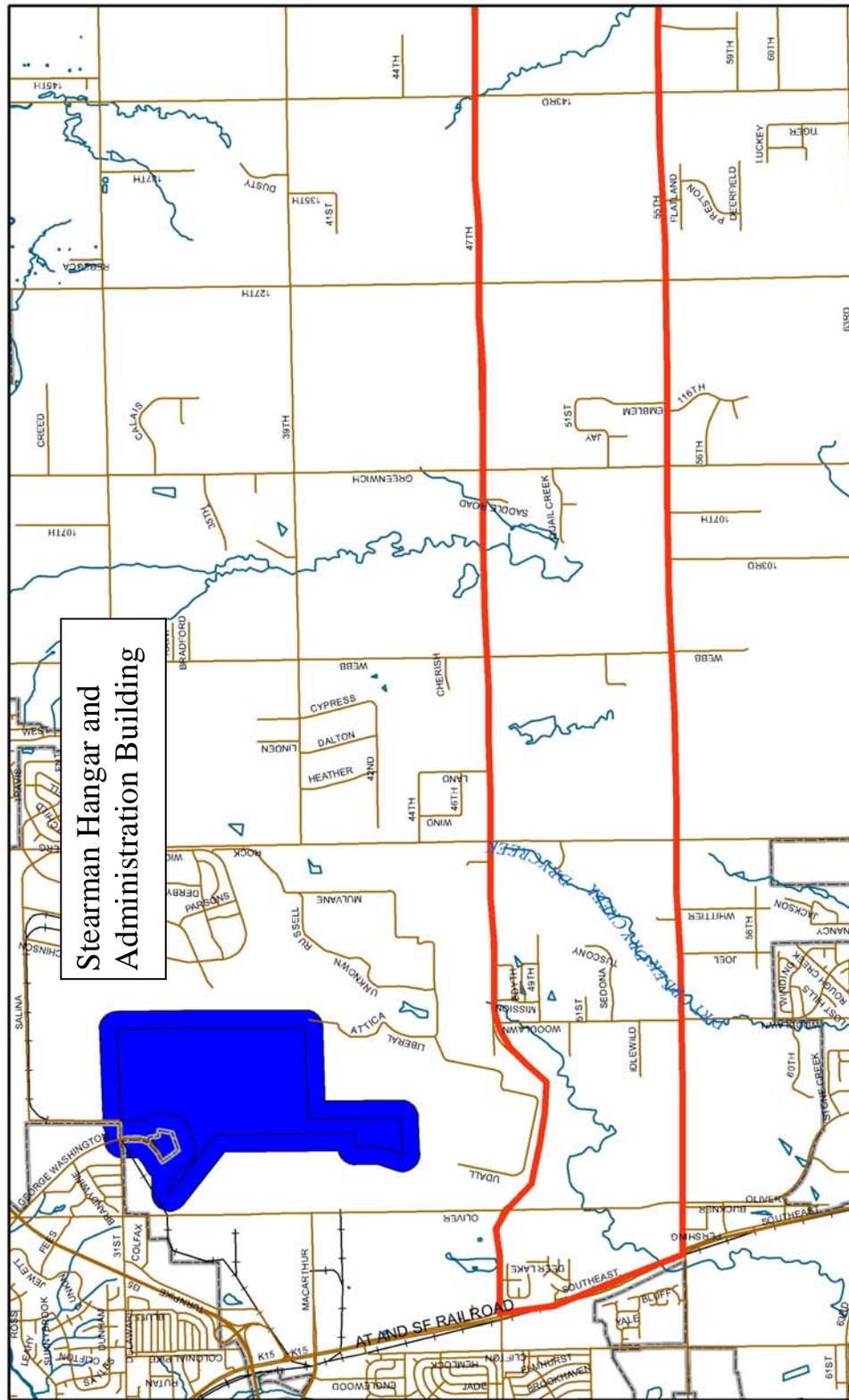
Administration Building, Municipal Airport

Construction of the Art Deco building began in 1929 and was completed in 1934 with the infusion of federal funds through New Deal relief programs. The facility provided municipal air service to Wichita until Mid-Continent Airport was constructed in 1951. The building and its grounds were then sold to the federal government. Special features include the stone panels on the front facade and the 37-foot cast Carthalite mural designed by L.W. Clapp. The mural depicts Charles Lindbergh's arrival at the coast of Ireland after crossing the Atlantic in 1927. The building is now home to the Kansas Aviation Museum.

Decommissioned Missile Silos

The plan area does not contain any decommissioned missile silos. The closest silo is located in eastern Butler County.

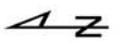
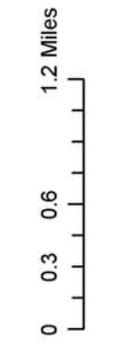
47th-55th Street South Joint Area Plan Historic Sites



**Stearman Hangar and
Administration Building**

Software: ArcGIS
Map Data Sources:
City of Wichita
Sedgewick County
Prepared: 01/27/2007

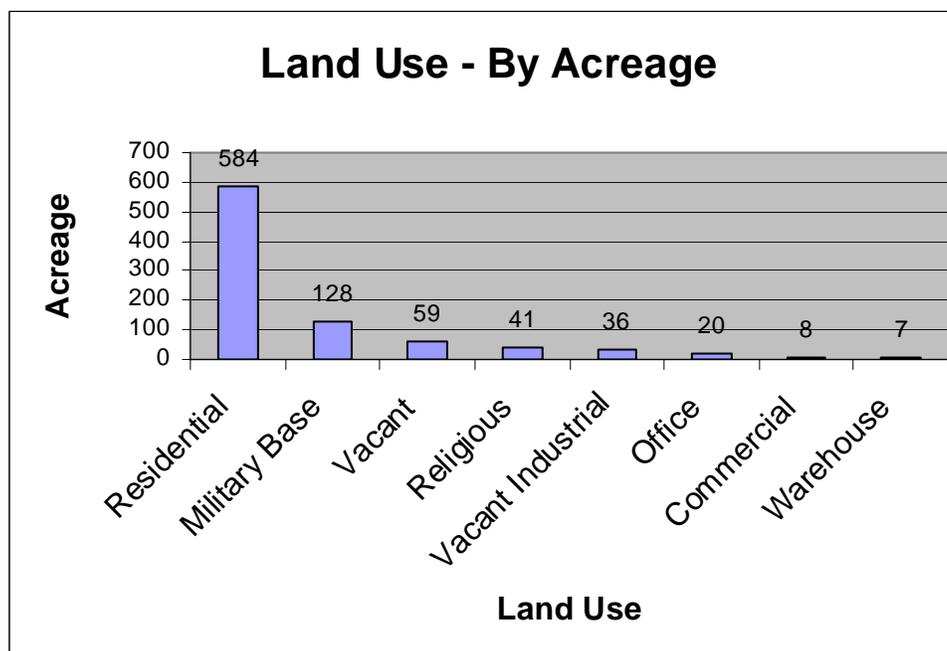
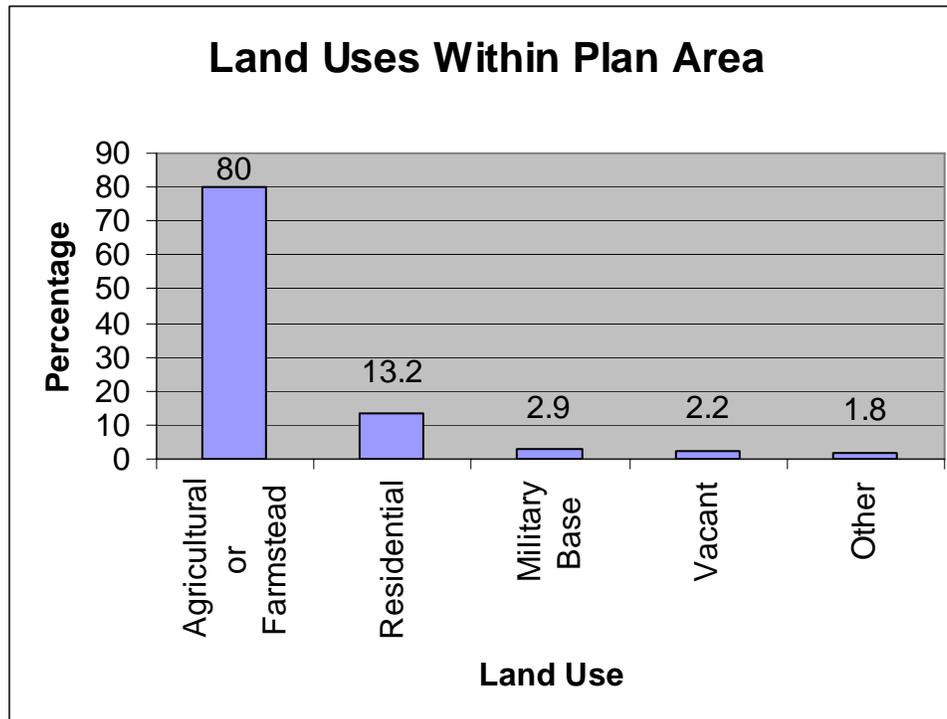
It is understood that while the City of Wichita Data Center Geographical Information Systems Department has indicated and reason to believe that there are inaccuracies in information incorporated in the base map, the Data Center GIS personnel make no warranty or representation, either expressed or implied, with respect to the information displayed. Other Note: Public property represented on this map is not intended to be inclusive.



- Plan Boundary
- National Historic Sites
- Regional Historic Sites

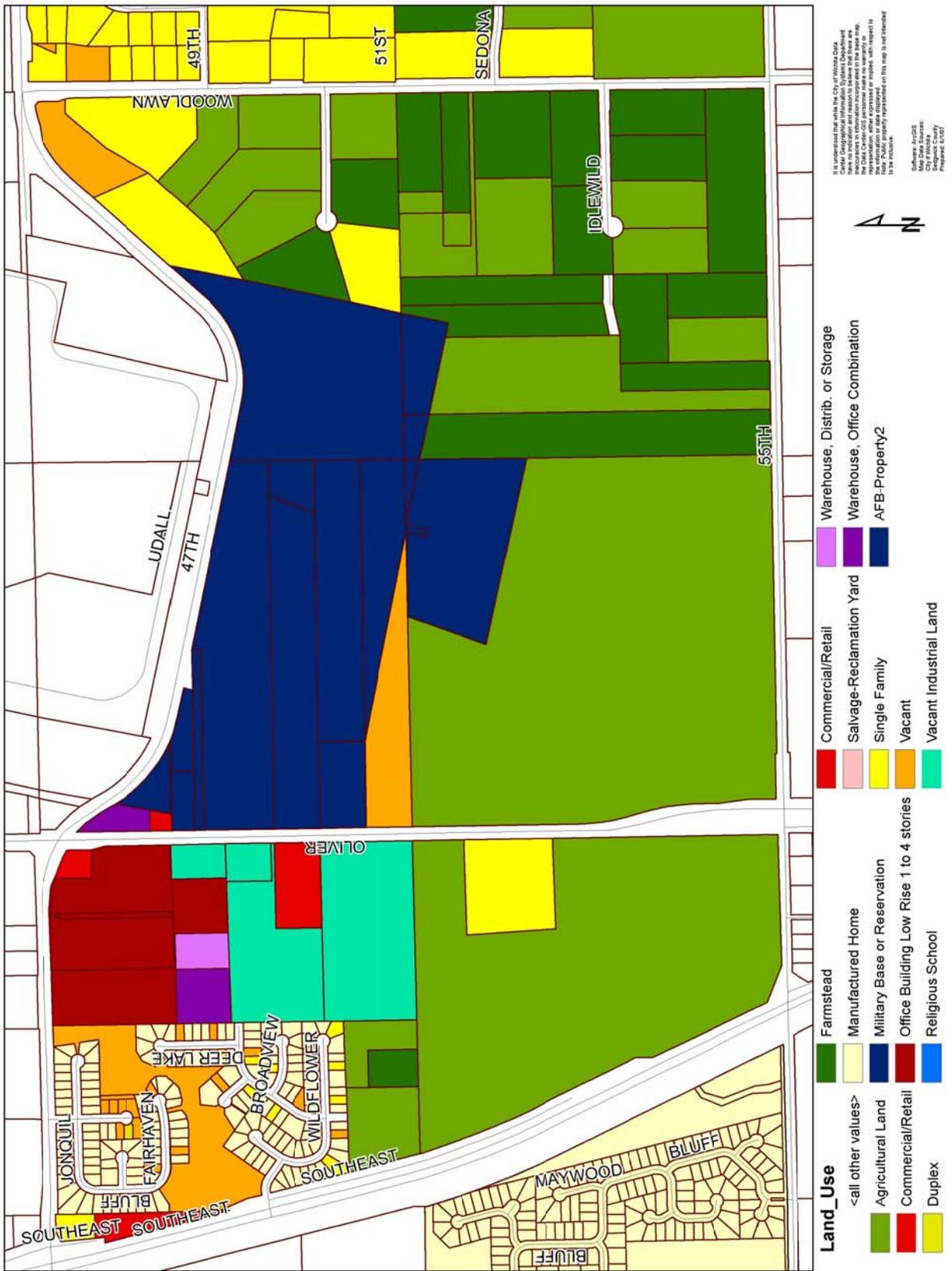
2.3 Existing Land Uses

The 47th-55th Street South Joint Plan Area consists primarily of agricultural land and farmsteads (80%), with some residential development (13%) scattered throughout the area. McConnell Air Force Base has approximately three percent (3%) of the overall land within the plan area. Warehouse, commercial retail, office, and religious institutions make up the “Other” category (1.8%).

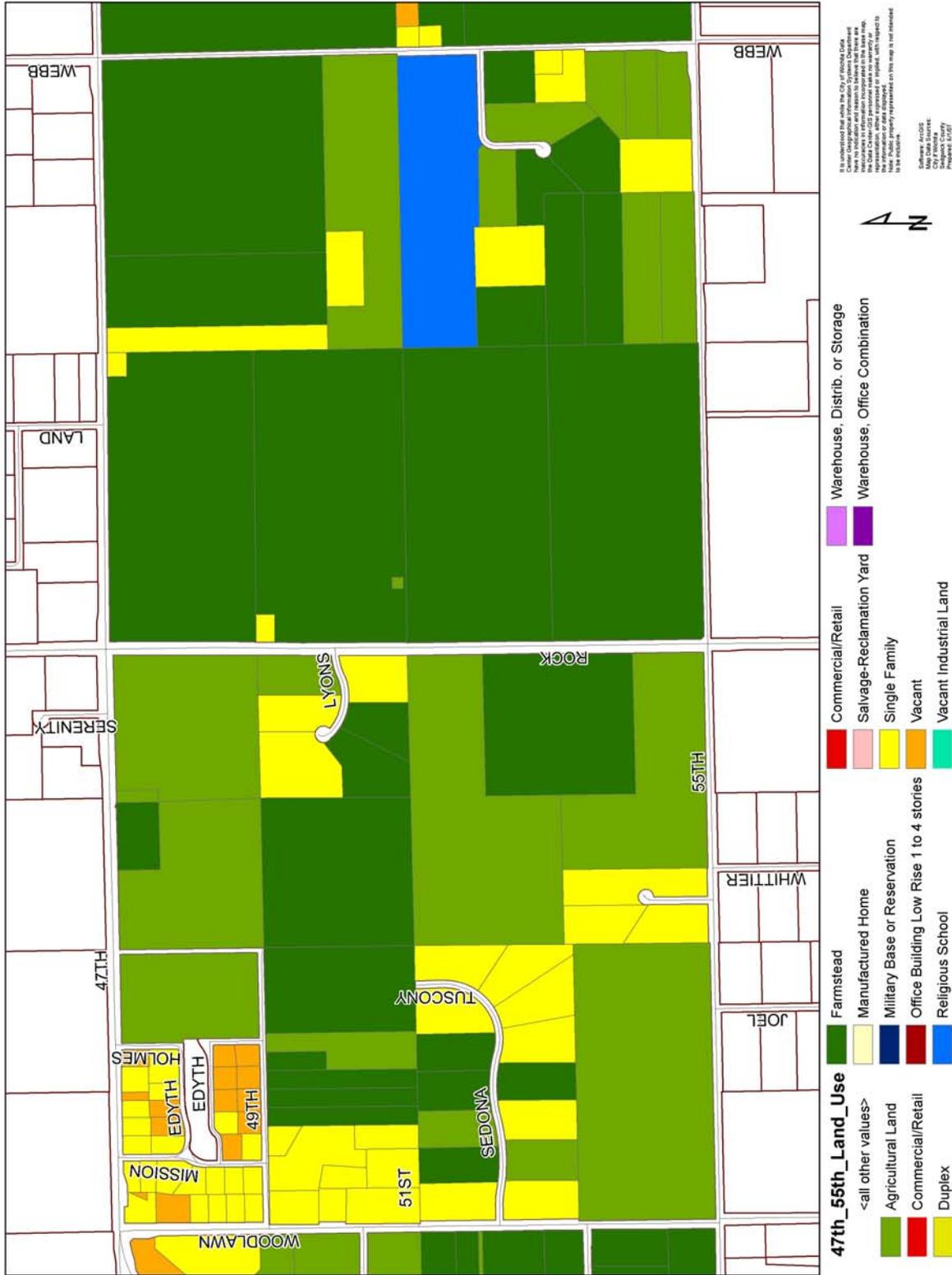


Map 9

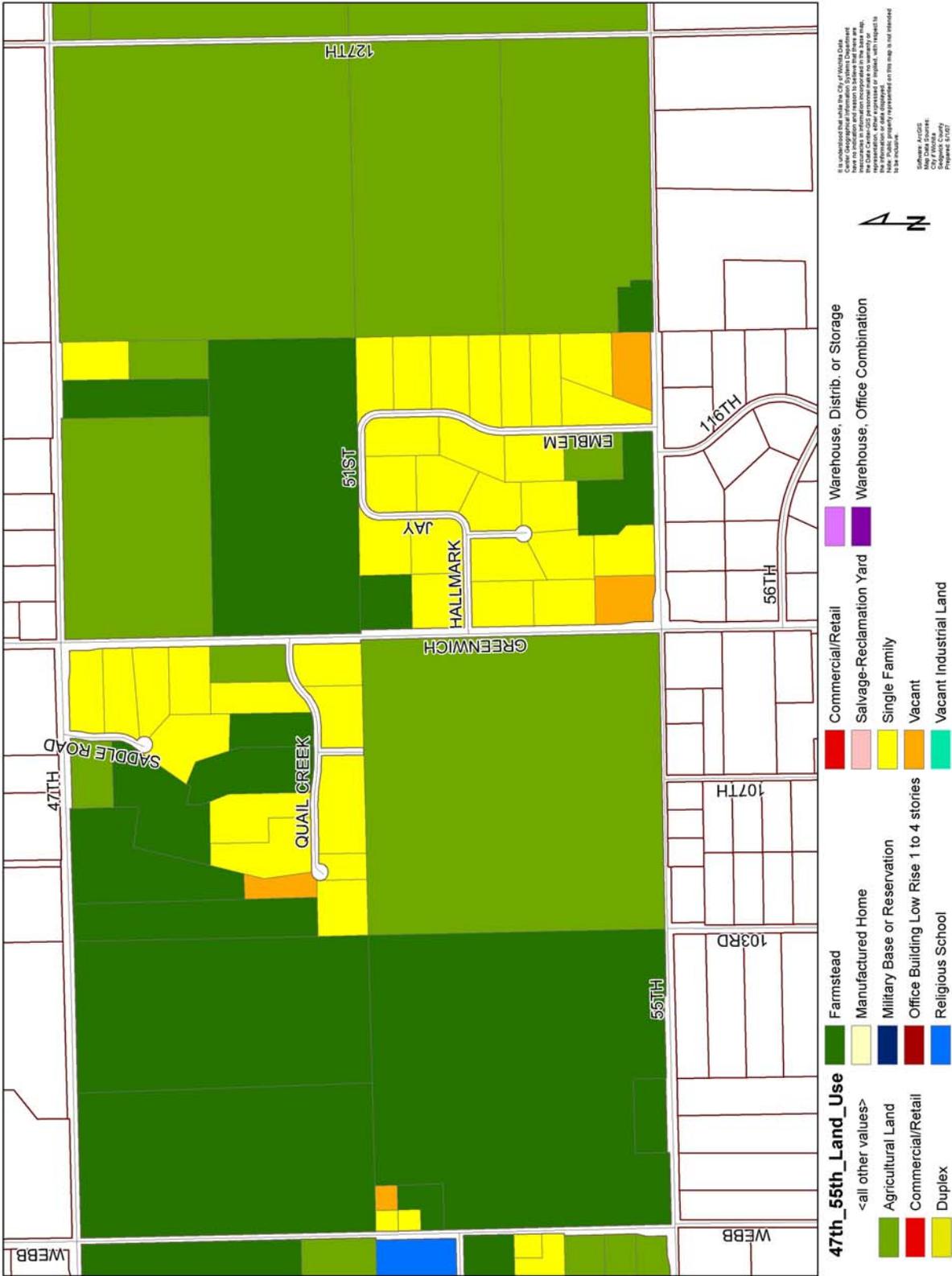
47th-55th Street South Joint Area Plan Land Use - K-15 to Woodlawn



47th-55th Street South Joint Area Plan
Land Use - Woodlawn to Webb Road



47th-55th Street South Joint Area Plan
Land Use - Webb Road to 127th Street



2.4 Development Trends

Land Ownership Patterns

There are sixteen property owners that own eighty (80) acres or more within the plan area, most of which are located east of Rock Road. West of Rock Road consists primarily of smaller parcels that have been subdivided over time.

Subdivision Activity

The majority of the subdivision activity throughout the plan area occurred in the 1970's and 1980's. The oldest subdivision, the Holmes Valley View Estates Addition was subdivided in the 1950's, which is located at the southeast corner of Woodlawn and 47th Street. This subdivision has experienced sporadic development with single-family homes built between 1938 and 1998, with several lots remaining vacant. The most recent subdivision activity within the plan area has occurred between Woodlawn and Rock Road in the late 1990's. Refer to Map #13 for further details.

Development Trend Data From 2002 to 2006

During the period 2002 to 2006 nineteen homes were built within the plan area. An additional four homes were constructed on the north side of 47th Street South and one home was built on the south side of 55th Street South during this period as well. Most of these homes were built in 2002 and 2003 as new subdivision regulations were coming into effect. Since then, rural residential construction has slowed considerably all over Sedgwick County including this area. Refer to Map #14.

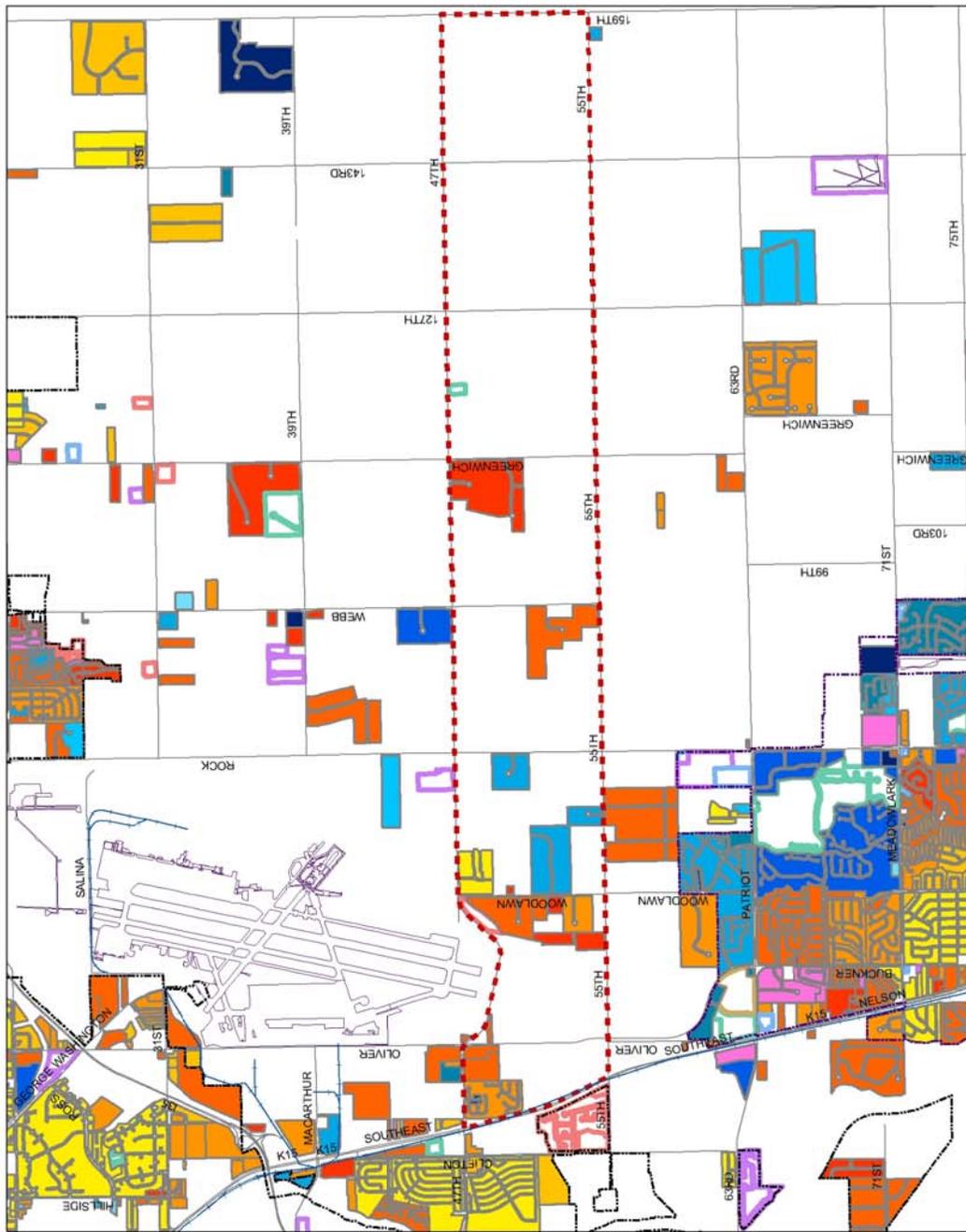
Only five commercial construction projects have been built within the plan area from 2002 to 2006. All five of these projects were built along 47th Street South and totaled \$2,134,000 in value. There were 44 commercial projects built at the fringe of the plan area with a value of \$9,520,000. All but two of these fringe projects took place at Boeing/ Spirit Aerospace facilities, east of the plan area. Refer to Map #15.

Overall Development Activity Prior to 2007

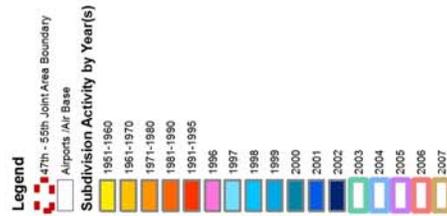
Most development has occurred within subdivisions or sporadically along paved arterials—with the majority of the development in the western portion of the plan area. A series of maps show the timeframe, in which development occurred by parcel: Map #16, Map #17, Map #18, and Map #19. (There are several parcels in which the Wichita-Sedgwick County GIS Departments do not have data available to show the year several structures were built; therefore, parcels that are white are either vacant or data was not available.)

Parcel Size

Since eighty percent (80%) of the plan area is either agricultural or farmsteads, there are several large parcels throughout the area. Since portions of the plan area as been sporadically subdivided, the parcel size ranges from .02 to 157 acres, with the Deer Creek Estates having the smallest lots. The average parcel size is 9 acres, but this number should be used carefully due to the wide range. Refer to the series of maps that show the acreage by parcel, Map #20, Map #21, Map #22, and Map #23.

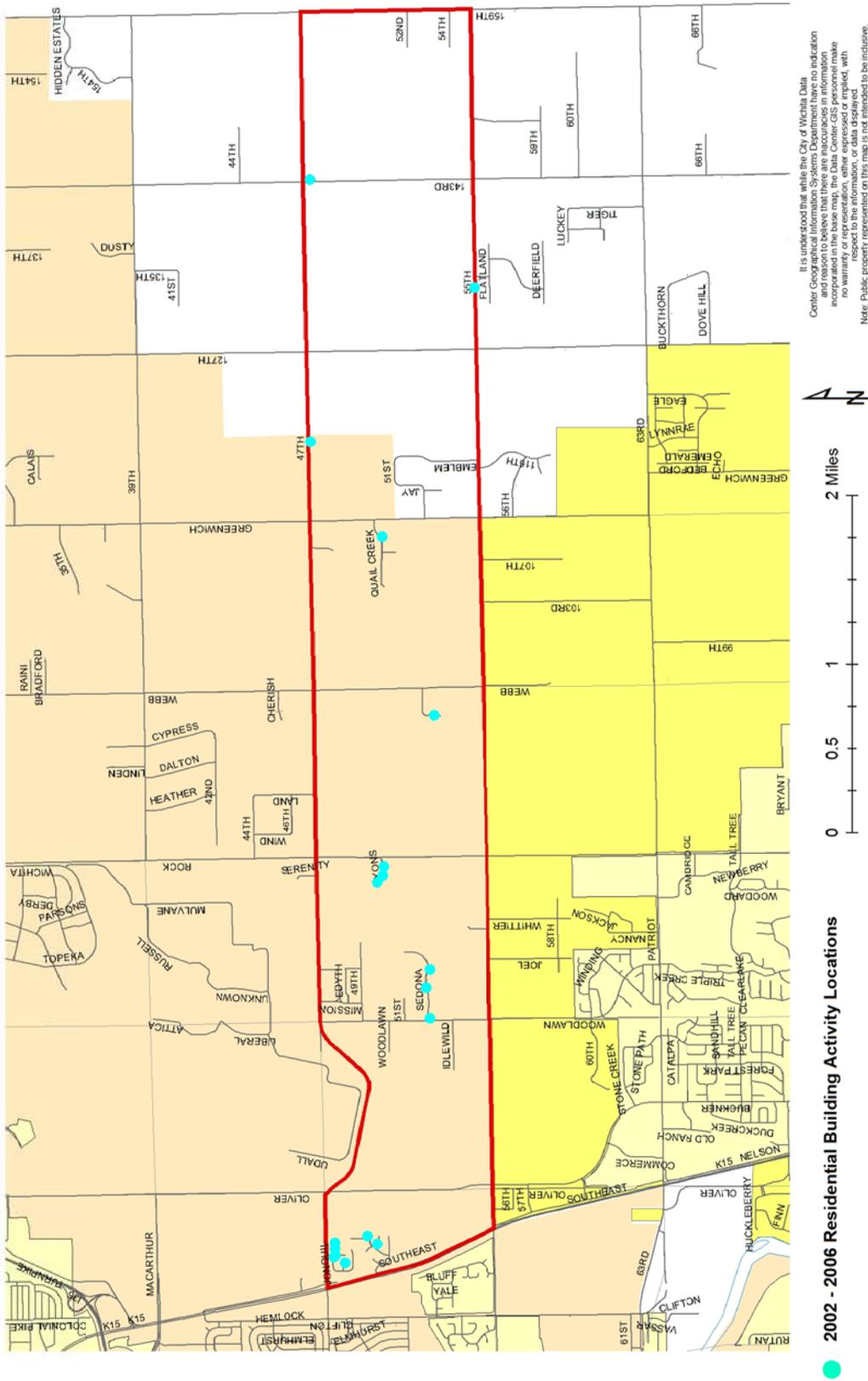


**45th-55th Street South
Joint Area Plan -
Subdivision Activity**



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**47th-55th Street South Joint Area Plan
2002-2006 Wichita and Sedgwick County
Residential Construction Activity**

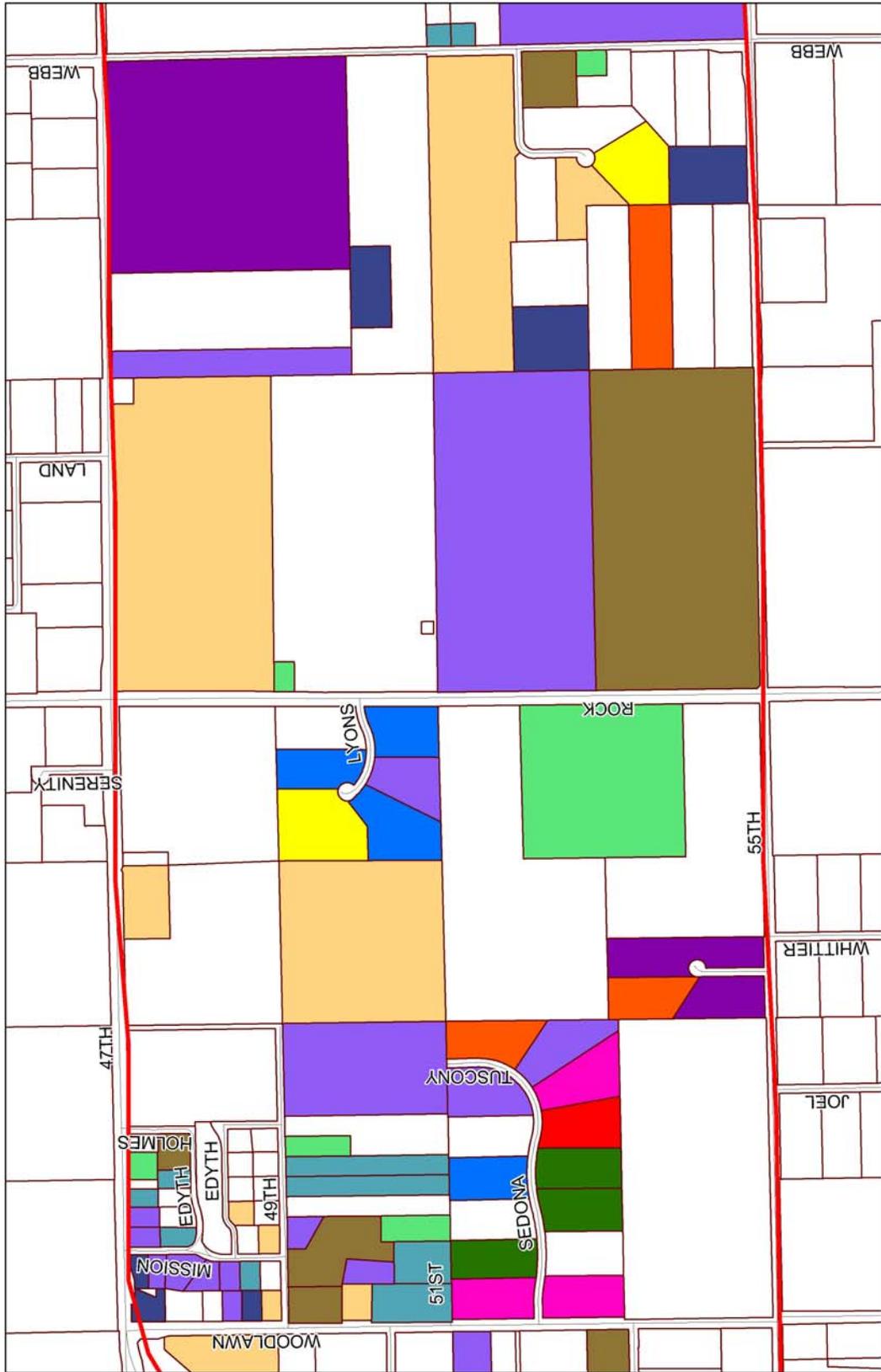


It is understood that while the City of Wichita Data Center Geographical Information Systems Department have no indication and reason to believe that there are inaccuracies in information incorporated in this base map, the Data Center GIS personnel make no warranty, expressed or implied, with respect to the information or data displayed.

Note: Public property represented on this map is not intended to be inclusive.

● 2002 - 2006 Residential Building Activity Locations

47th-55th Street South Joint Area Plan
Development Activity - Woodlawn to Webb Road

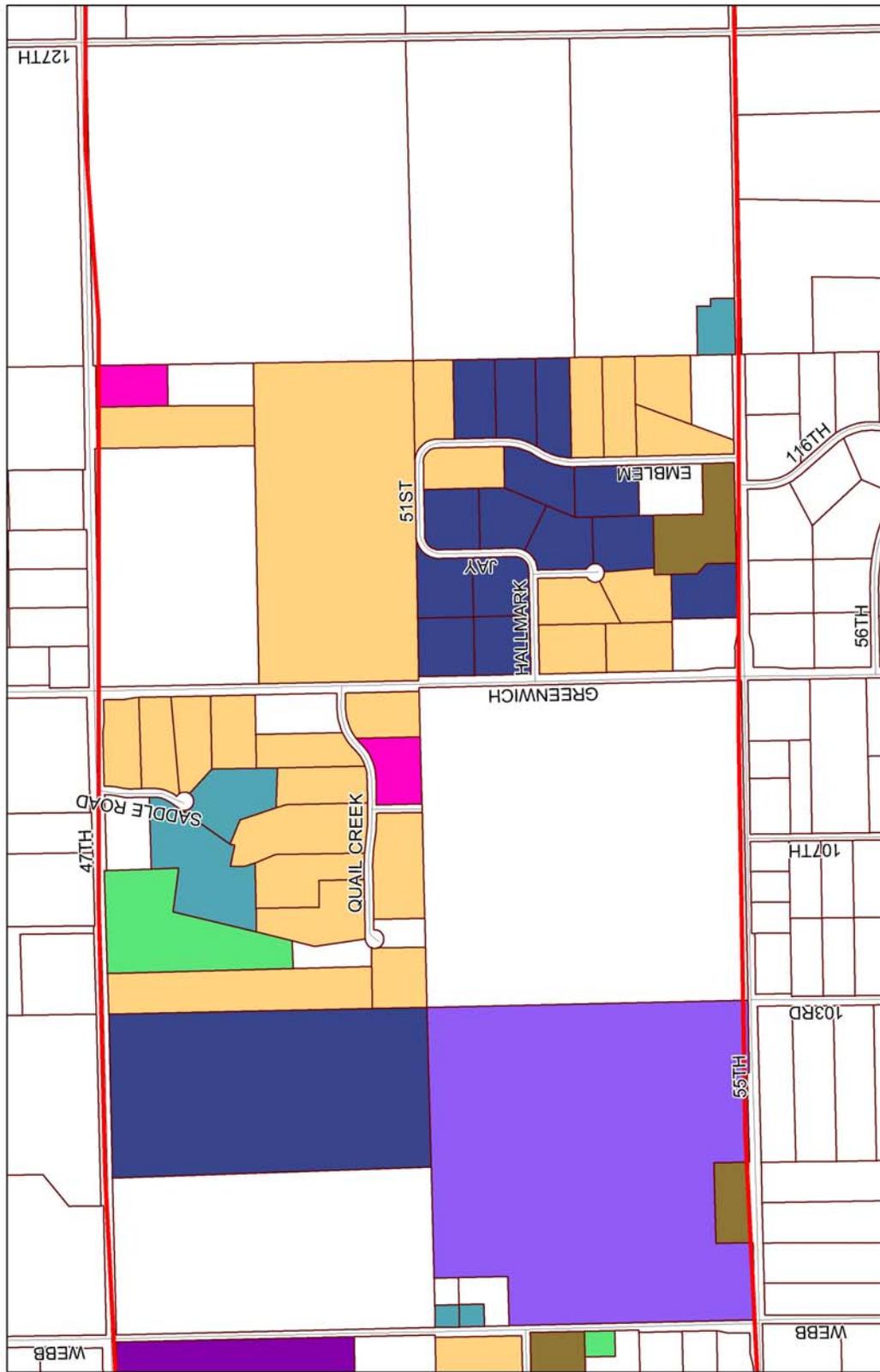


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- Year Built**
- 1960 to 1969
 - 1970 to 1979
 - 1980 to 1989
 - 1990 to 1999
 - 1949 or earlier
 - 1950 to 1959
 - 2000
 - 2001
 - 2002
 - 2003
 - 2004
 - 2005
 - 2006
 - Plan Area

47th-55th Street South Joint Area Plan
 Development Activity - Webb Road to 127th Street

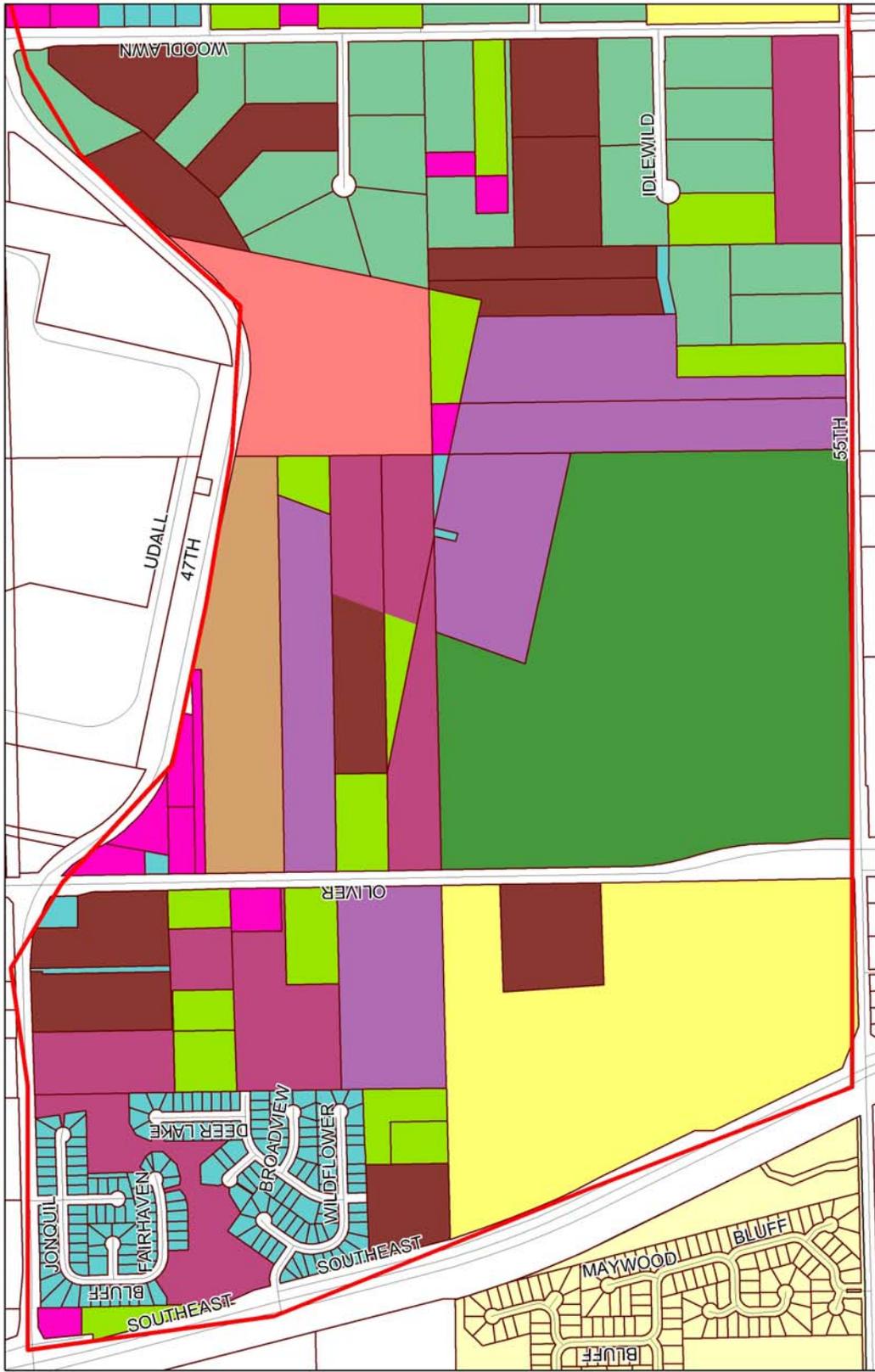


Software: ArcGIS
 Map Data Source:
 City of Stoughton
 Stoughton County
 Project: 11/07
 Note: Public property represented on this map is not intended to be accurate.



- Year Built**
- 1960 to 1969
 - 1970 to 1979
 - 1980 to 1989
 - 1990 to 1999
 - 2000
 - 2001
 - 2002
 - 2003
 - 2004
 - 2005
 - 2006
- Plan Area**
- Plan Area

**47th-55th Street South Joint Area Plan
Acreeage - K-15 to Woodlawn**



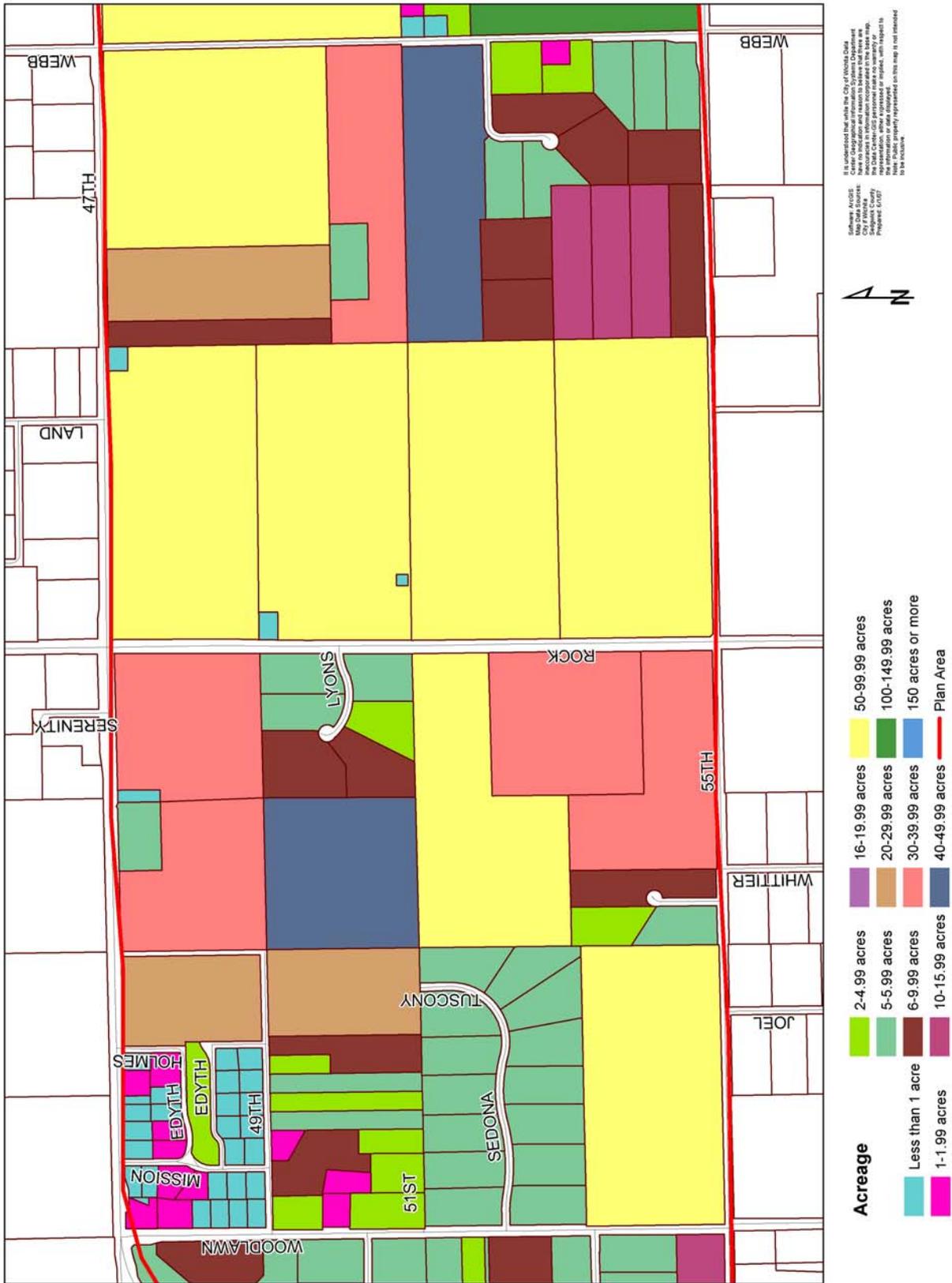
Software: ArcGIS
 Data: Geographical Information System Data
 City of Yuba
 Planning Department
 Project: K-15
 The City of Yuba does not warrant the accuracy of the information or data displayed on this map. Property boundaries are not guaranteed.



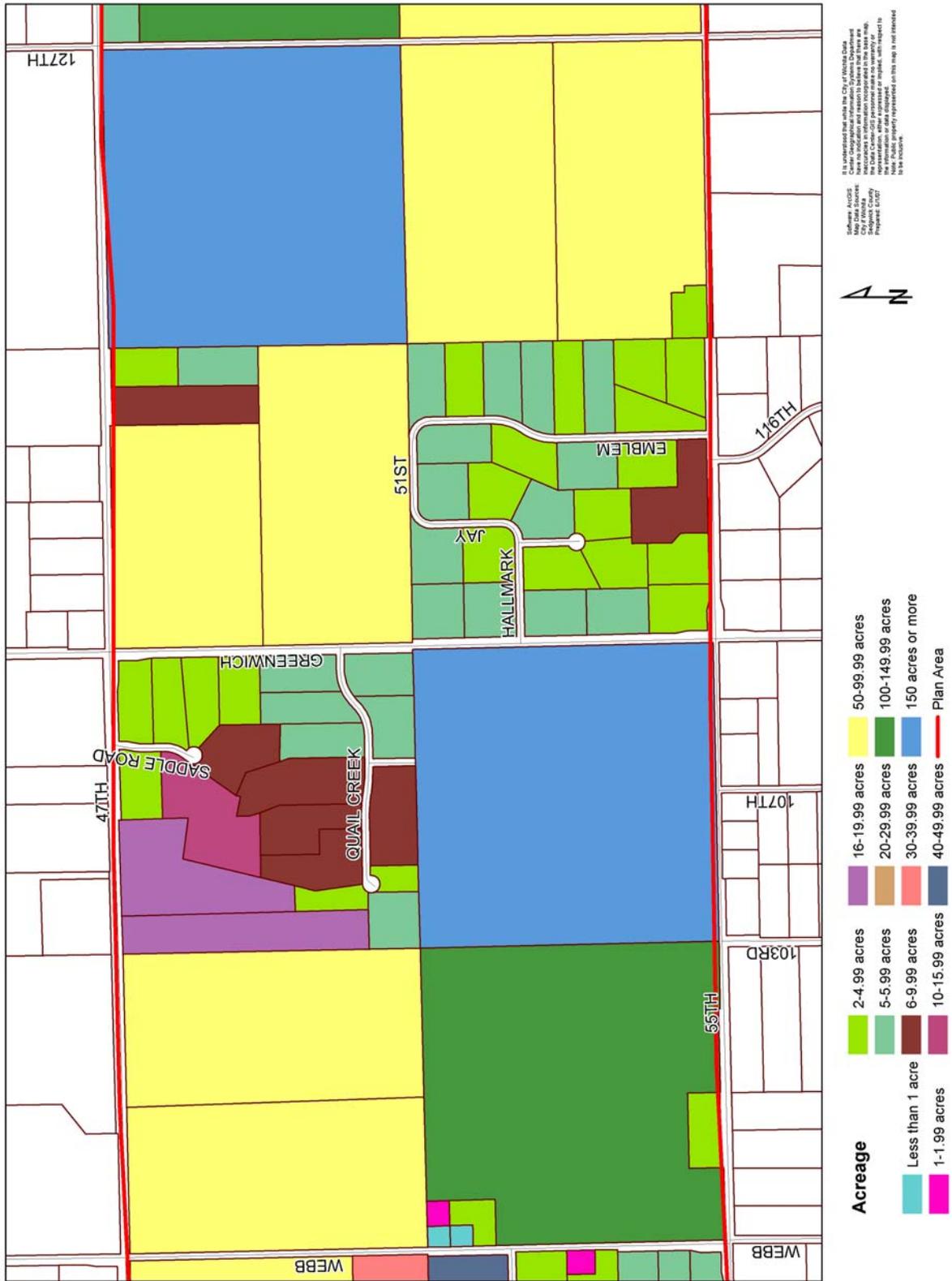
Acreeage

Less than 1 acre	1-1.99 acres	2-4.99 acres	16-19.99 acres	50-99.99 acres
1-1.99 acres	5-5.99 acres	6-9.99 acres	20-29.99 acres	100-149.99 acres
1-1.99 acres	10-15.99 acres	30-39.99 acres	40-49.99 acres	150 acres or more
1-1.99 acres	10-15.99 acres	40-49.99 acres	40-49.99 acres	Plan Area

**47th-55th Street South Joint Area Plan
Acrage - Woodlawn to Webb Road**



**47th-55th Street South Joint Area Plan
Acreege - Webb Road to 127th Street**



2.5 Existing Services

Law Enforcement

Sedgwick County Sheriff Patrol Beat #4 covers almost the entire plan area from Oliver east to 159th Street East. Sedgwick County Sheriff Patrol Beat #5 covers a small section of the plan area, between K-15 east to Oliver Street. The City of Wichita is divided into North, East, South, and West Bureaus with a series of Beats within each Bureau. Wichita's South Bureau currently serves the area directly west of the plan area—just west of K-15. The City of Derby's Police serve within the Derby City Limits—directly south of the plan area. Refer to Map #24 & 25.

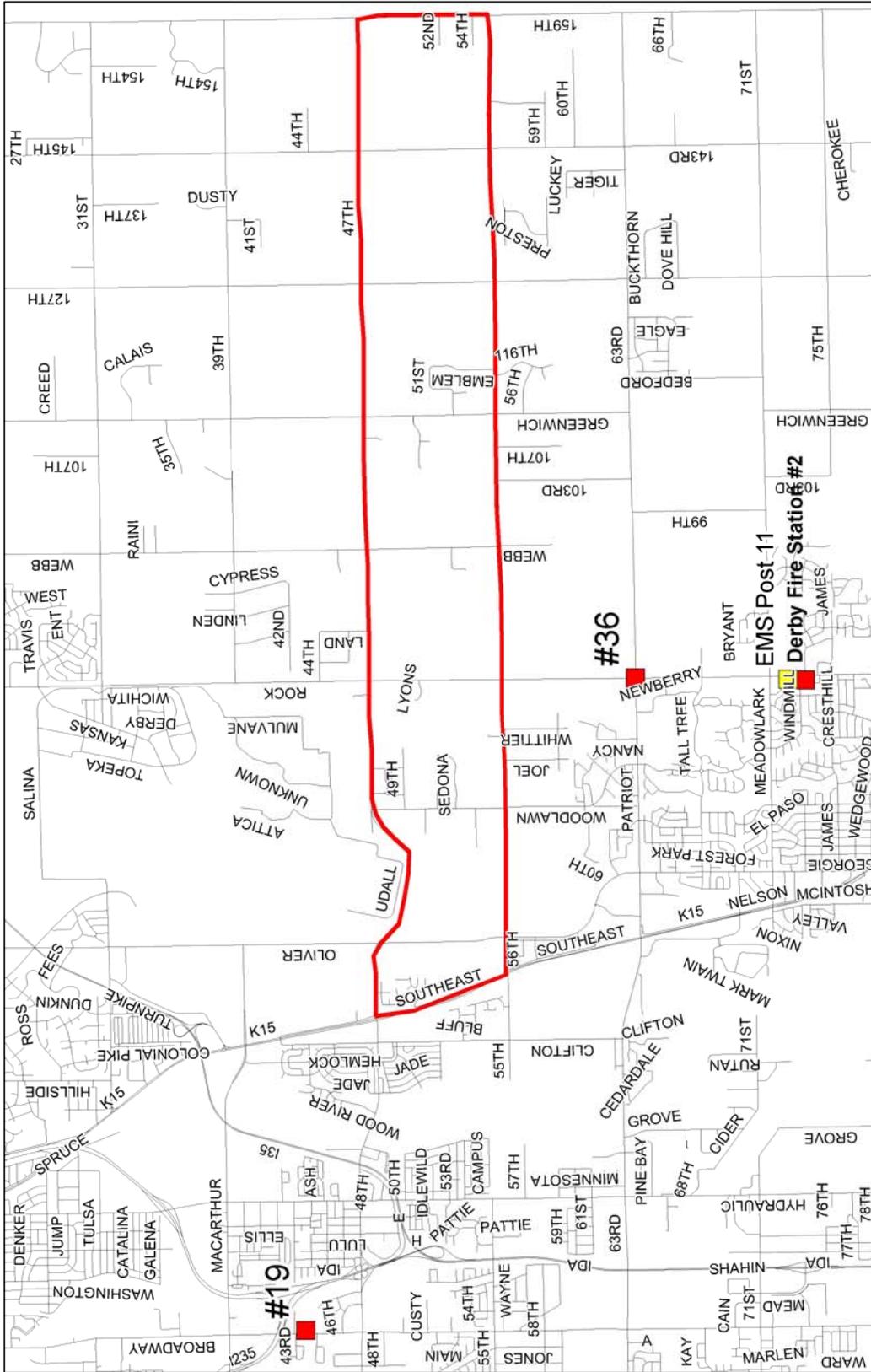
Fire

The plan area is within the Sedgwick County Fire District #1 and is primarily served by the Sedgwick County Fire Station #36, located one-mile south of the plan area at 63rd Street and Rock Road. The City of Wichita and Sedgwick County have a first-responder agreement in place, so depending on the location of the fire, the City of Wichita Fire Station #19, located approximately two miles west of the plan area (4440 South Broadway), and if needed, the City of Derby's Fire Station #2 located at 71st Street and Rock Road, could also serve the plan area. Depending on the location of a fire, fire response times could vary from three (3) to fifteen (15) minutes. Refer to Map #26.

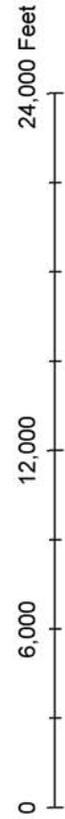
EMS

Sedgwick County EMS Post #11, located at 71st Street South and Rock Road, serves the plan area, with estimated response times ranging from six (6) to eighteen (18) minutes. In addition, ambulance service to Derby is provided by Sedgwick County EMS. Currently, agreements with Sedgwick County specify the City of Derby Fire Department to serve as the first medical responders with City Emergency Medical Technicians (EMT), which provide support for Sedgwick County EMS providers. Refer to Map #26 to view the location of EMS Post #11.

47th-55th Street South Joint Area Plan Fire & EMS Stations



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- Fire Stations
- Emergency Medical Services

Water and Sewer Service

Rural Water District

Sedgwick County Rural Water District #3 serves the majority of the plan area, from about Woodlawn east to the Butler County Line. By State Statute, when any city annexes properties that are served by a Rural Water District (RWD), that City must enter into good faith negotiations with the RWD to acquire that portion of their system. Please refer to Map #27.

City of Wichita Water and Sewer Service

The City of Wichita serves only a small portion of the plan area. Wichita serves the Deer Lake Estates subdivision, at the southeast corner of K-15 and 47th Street South. Wichita also maintains a small sewer system southeast of Greenwich Road and 63rd Street South. Refer to Map #28.

Existing Agreements Between Derby and Wichita

The City of Derby has a service agreement with the City of Wichita for Derby to provide water service south of 55th Street primarily from K-15 to Greenwich Road. Therefore, at this time, the City of Derby has no plans to serve any area north of 55th Street South. Refer to Map #29 to review Derby's current service area boundaries according to the agreement.

Recent Water Improvements by the City of Derby

A 2002 Water Study recommended the construction of a new 1.5 MG water tower at 63rd Street South and Rock Road, which has been constructed and has the capacity to serve "high elevation" areas generally consisting of Derby's northeast growth area in the Spring Creek drainage basin, including the Derby High School, and all areas to the north on the east side of Rock Road, the new golf course, and generally all other areas to the north of a line about one-half mile north of Meadowlark. The water tower also benefits the entire city by providing increased pressure to meet the demands of a growing population in the southern part of the city.

Derby's Water Supply

The water system serving Derby was owned and operated by the El Paso Water Company until it was purchased by the City of Derby in 2001. This acquisition was important because it meant the Company was no longer in business to generate profits for company stockholders from Derby residents. The Derby City Council currently acts as the Board of Directors of the company, which enables the City to ensure that revenues from water sales are set at levels to cover expenses, rather than set at a level to provide income for investors.

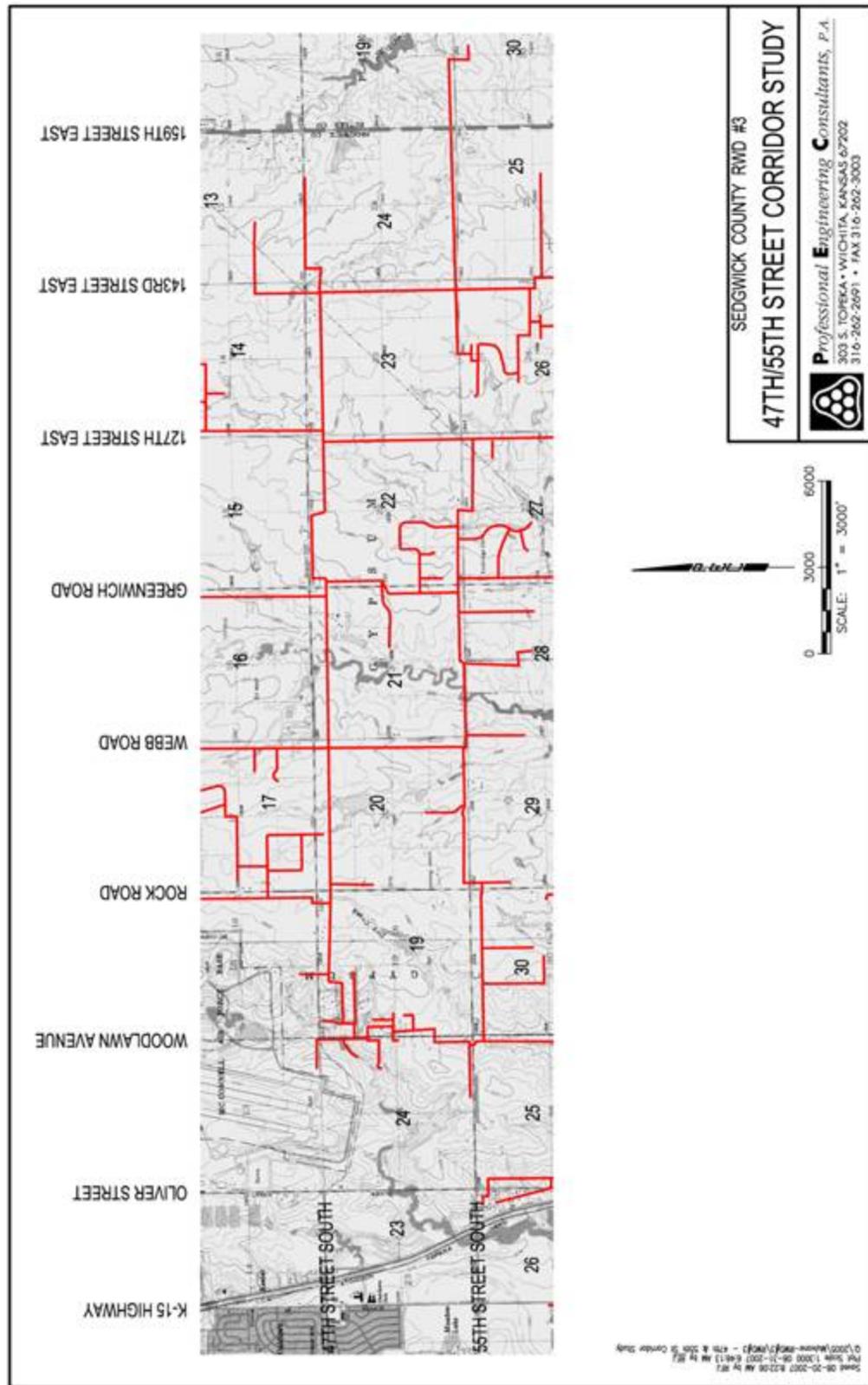
In years past, the water supply source consisted of a water well field located west of the Arkansas River. Due to concerns about water quality and about the ability of the well field to meet the community's future water needs, an agreement was reached in 2001 for the City of Derby to purchase 100 percent of its treated water from the City of Wichita. The first full year of utilizing 100 percent Wichita water for Derby's water supply began in 2005.

Although Derby's current water usage is entirely supplied from the Wichita system, the City of Derby maintains ownership rights of 700 million gallons of water per year from the El Paso system, which are not currently being used. These water rights represent a tremendous asset for Derby either as a future water source or as a possible revenue source.

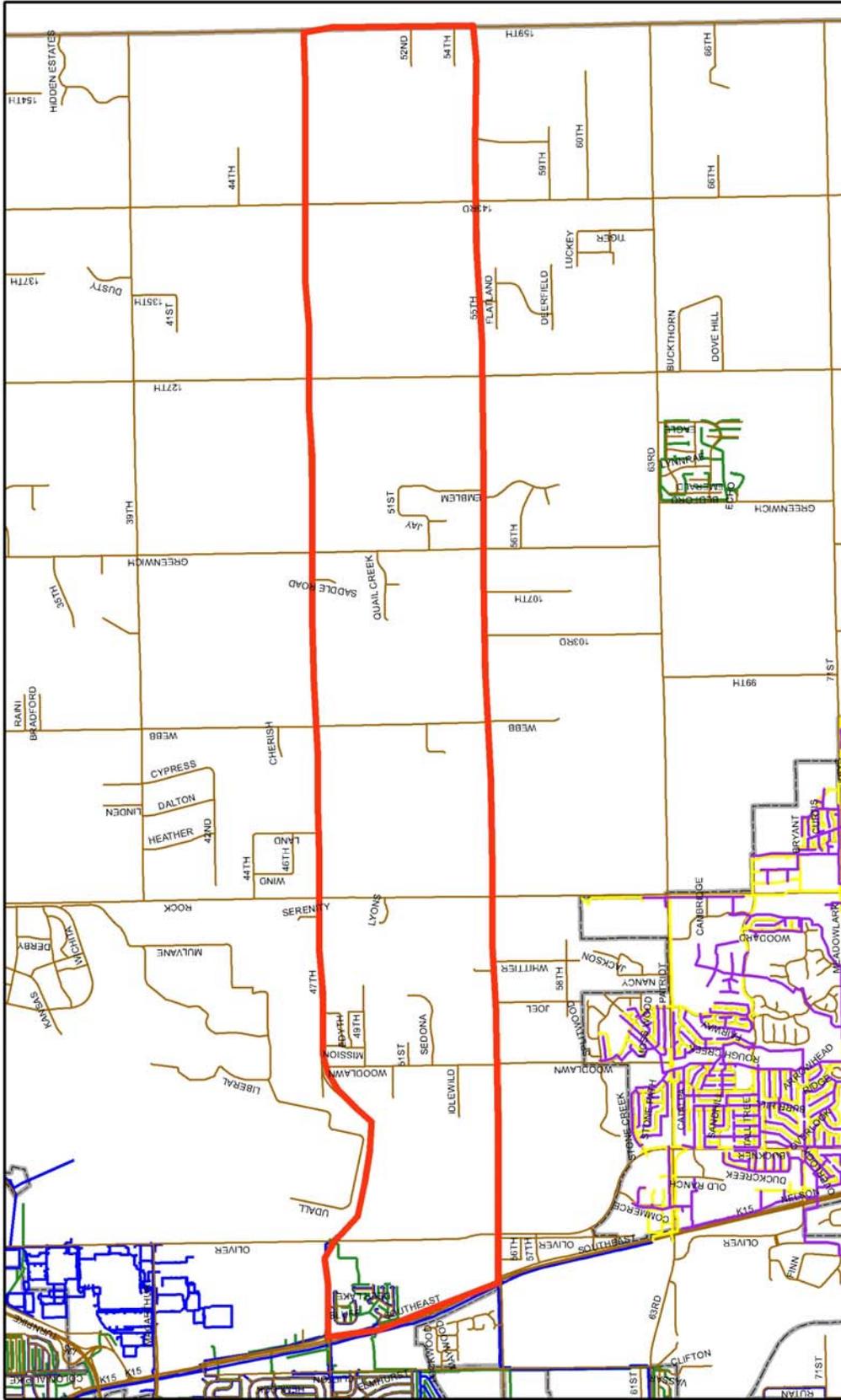
Electric

Weststar Energy serves the plan area primarily from K-15 east to about one-half mile east of Greenwich Road. Weststar also has a line that runs along 55th Street to about one-half mile east of 127th Street that serves properties south of 55th Street. Butler Rural Cooperative serves the plan area primarily between 127th and 159th Street South. Refer to the Weststar Map #30.

47th-55th Street South Joint Area Plan Sedgwick County Rural Water District #3 Lines



47th-55th Street South Joint Area Plan Existing Municipal Water and Sewer Lines



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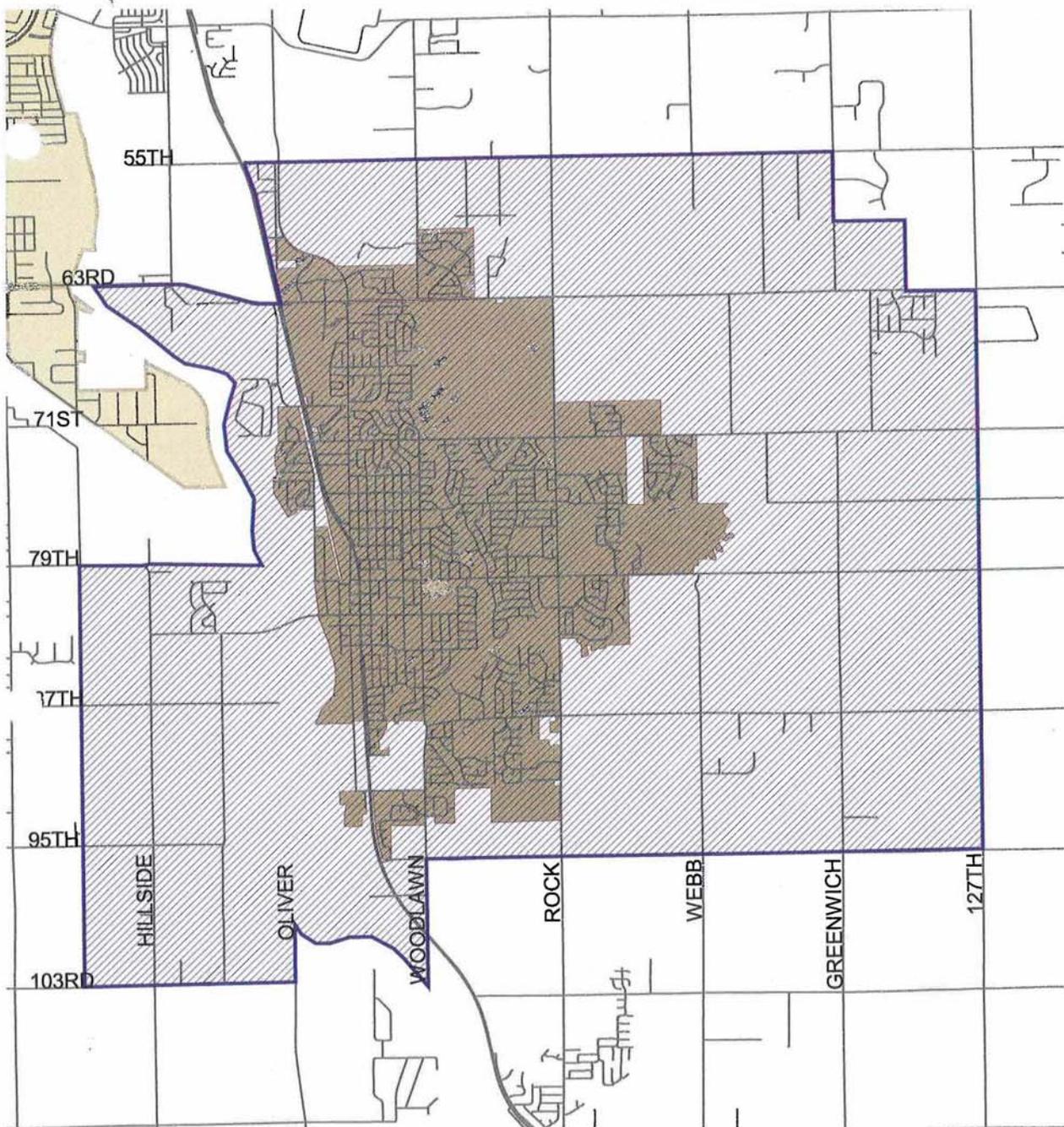
Software: ArcGIS
 Map Data Sources:
 City of Wichita
 Sedgwick County
 Prepared: 6/27/2007

0 0.45 0.9 1.8 Miles

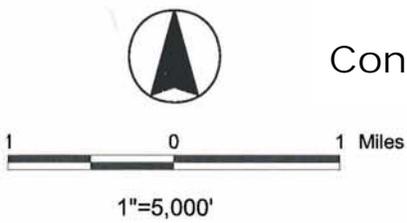
N

- Plan Boundary
- Wichita Water Lines
- Wichita Sewer Lines
- Derby Water Lines
- Derby Sewer Lines

Map 29



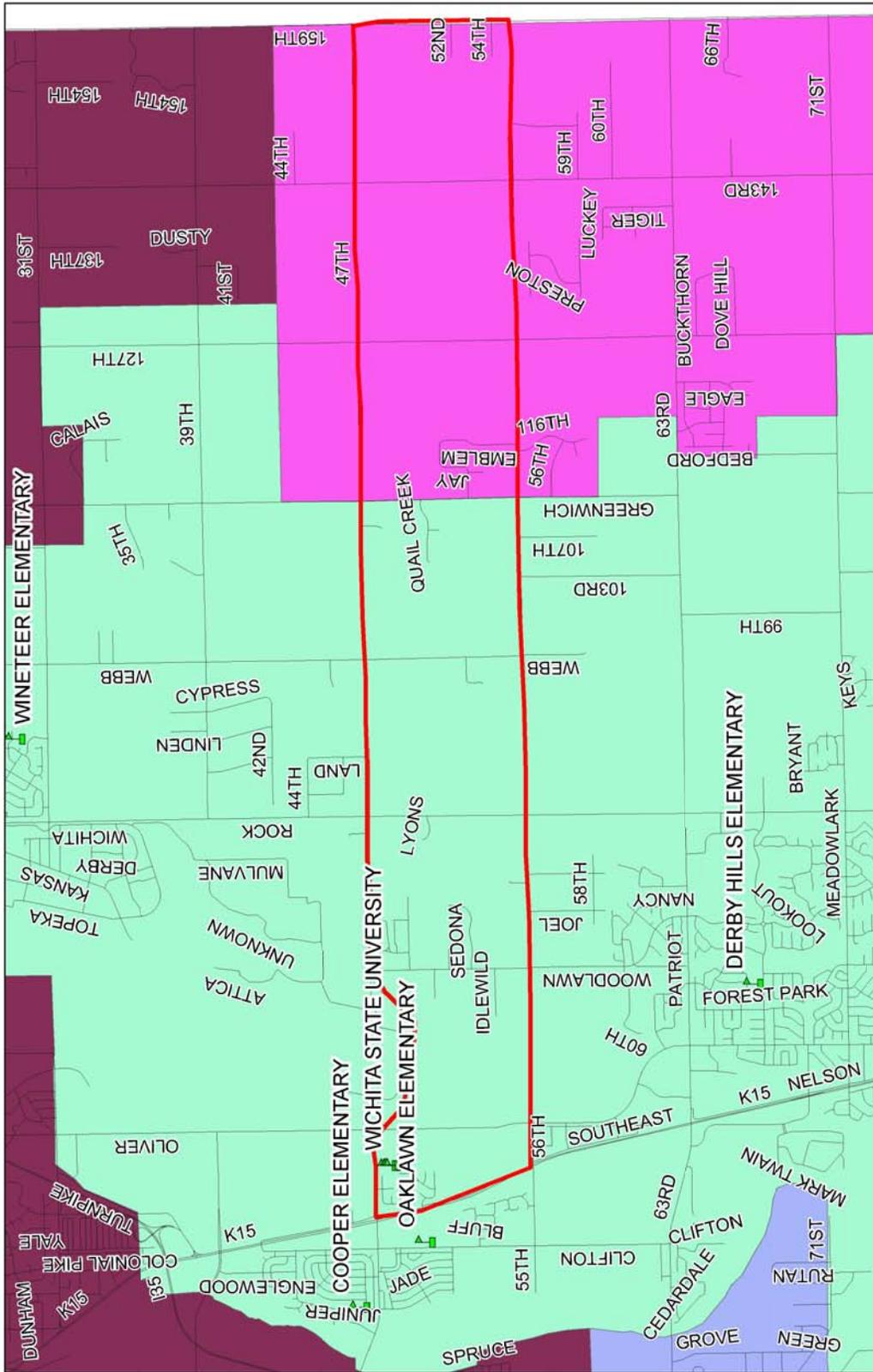
City of Derby
Contract Water Service Area



-  Derby Service Area
-  Streets
-  City Limits
-  Derby
-  Wichita

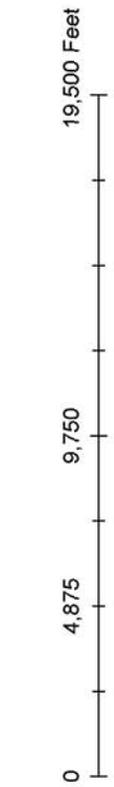


**47th-55th Street South Joint Area Plan
Unified School Districts & Schools**



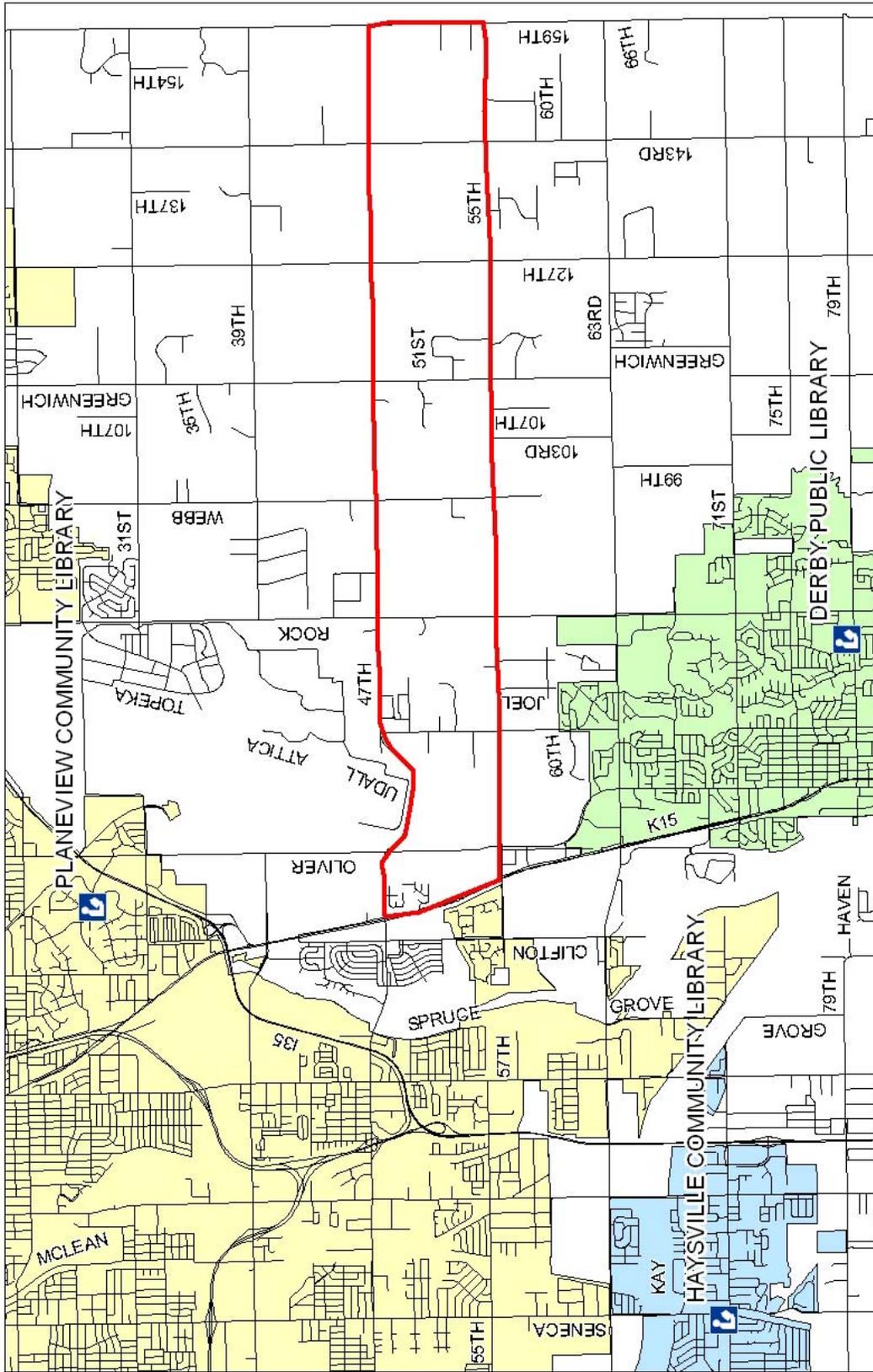
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Software: ArcGIS
City of Wichita
Planning & Development
Project: 201001

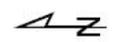


- USD NAME**
- DERBY
 - HAYSVILLE
 - ROSE HILL
 - WICHITA
 - Schools

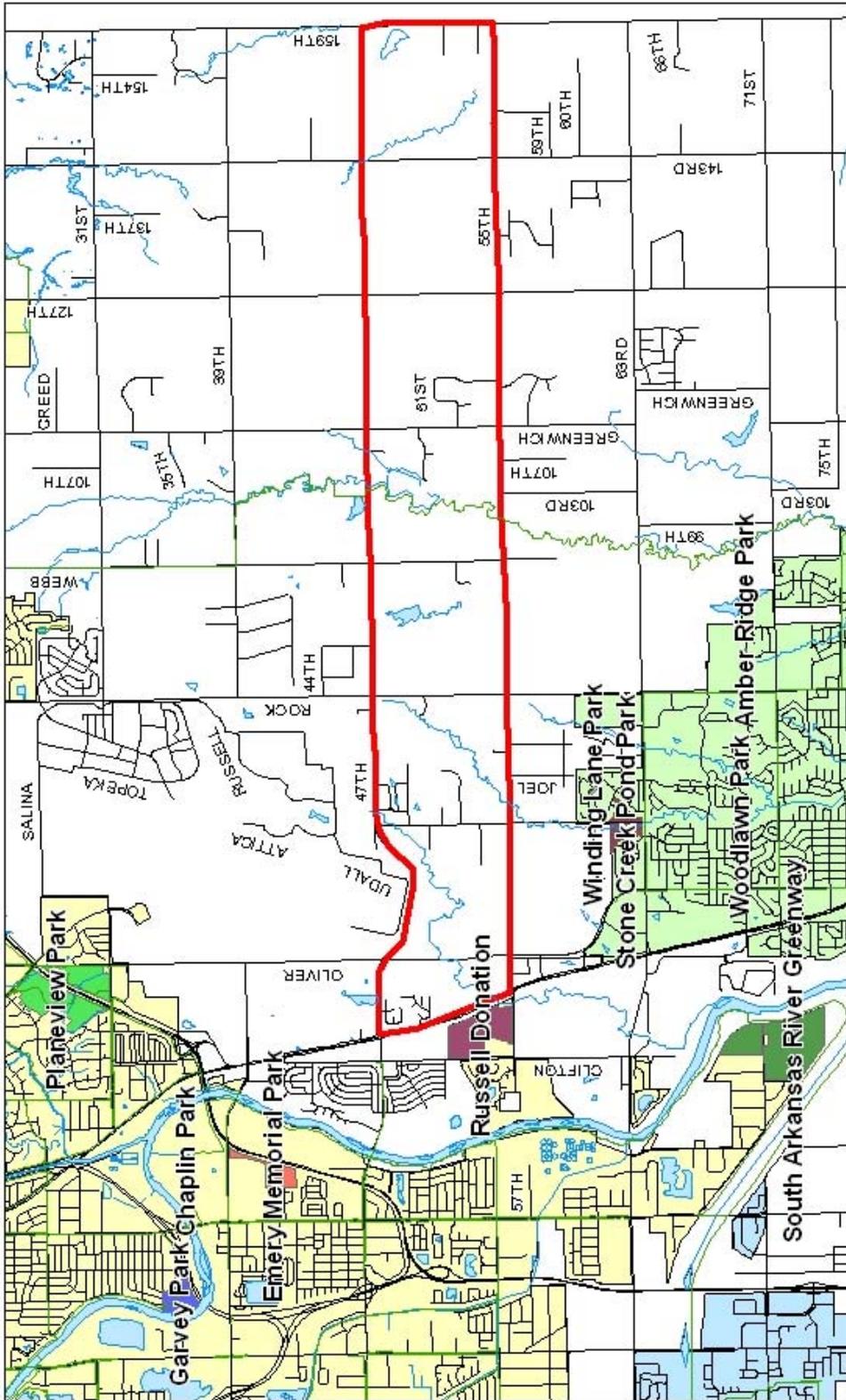
47th-55th Street South Joint Area Plan Libraries



Map prepared for the City of Wichita, Kansas
 Source: City of Wichita, Kansas
 Date: 10/20/2017
 The City of Wichita, Kansas
 Planning Department
 1000 North Broadway
 Wichita, Kansas 67202
 Phone: 316.251.2000
 Fax: 316.251.2001
 Website: www.wichita.gov



47th-55th Street South Joint Area Plan
Park and Open Space

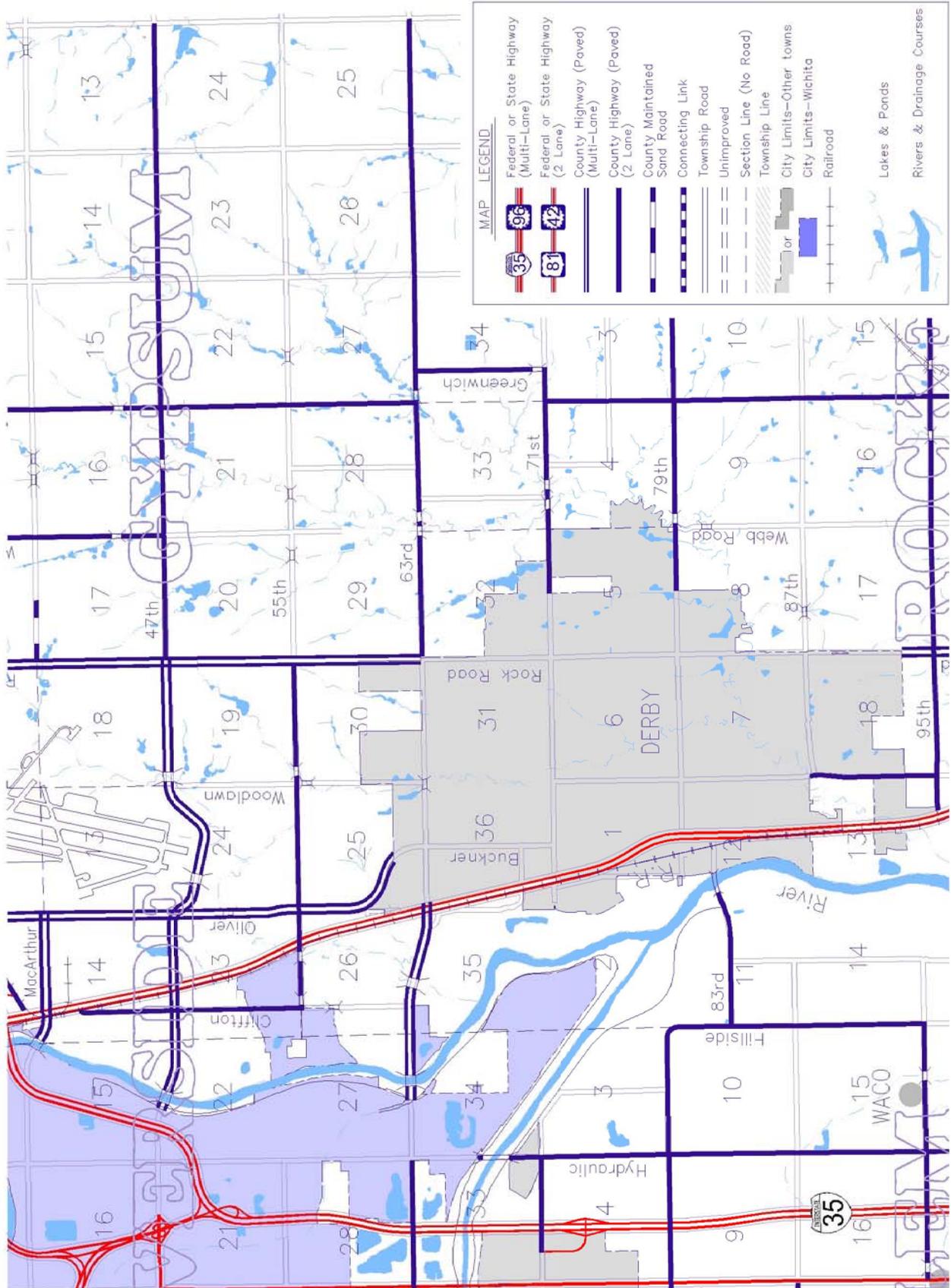


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Data Sources: City of Oklahoma City, Oklahoma County
Prepared: 1/22/08



- 4756P lan
- DERBY
- HAYSVILLE
- WICHITA
- Parks
- Emery Memorial Park
- Garvey Park
- Planeview Park
- Dusk Creek Park
- Amber Ridge Park
- Chaplin Park
- South Arkansas River Greenway
- Stone Creek Pond Park
- Stone Creek Pond Park
- Winding Lane Park
- Woodlawn Park
- Hike & Bike Paths

47th-55th Street South Joint Area Plan Road Maintenance



2.6 Existing Zoning and Land Use Policies

Existing Zoning

The existing zoning within the plan area is primarily “RR” Rural residential and “SF-20” Single-Family Residential, with some “SF-5” Single-Family Residential, “MH” Manufactured Housing, “LC” Limited Commercial, “LI” Limited Industrial, “AFB” Air Force Base, “IP-A” Industrial Park—Airport. The most recent change to the plan area includes the addition of an Air Force Base Zoning District, an Air Force Base Protection Overlay District, a McConnell Airport Overlay District, and an “IP-A” Industrial Park—Airport zone. (See below for further details) Please note that the Wichita-Sedgwick County Unified Zoning Code and the Derby Zoning Code are two different zoning codes. Since the plan area is within the County, the County’s zoning applies to this area. Refer to Map #35.

Impacts of the McConnell AFB Joint Land Use Study on the Plan Area

Both the Wichita-Sedgwick County Comprehensive Plan and the City of Derby Comprehensive Plan reflect implementation recommendations contained in the McConnell Air Force Base Joint Land Use Study. These recommendations will:

- Have an impact on how the future of the plan area is developed
- Protect and promote the present and future operational/mission capacity of the McConnell AFB
- Promote the long-term health, safety and welfare of the civilian and military community associated with, and in proximity to McConnell AFB

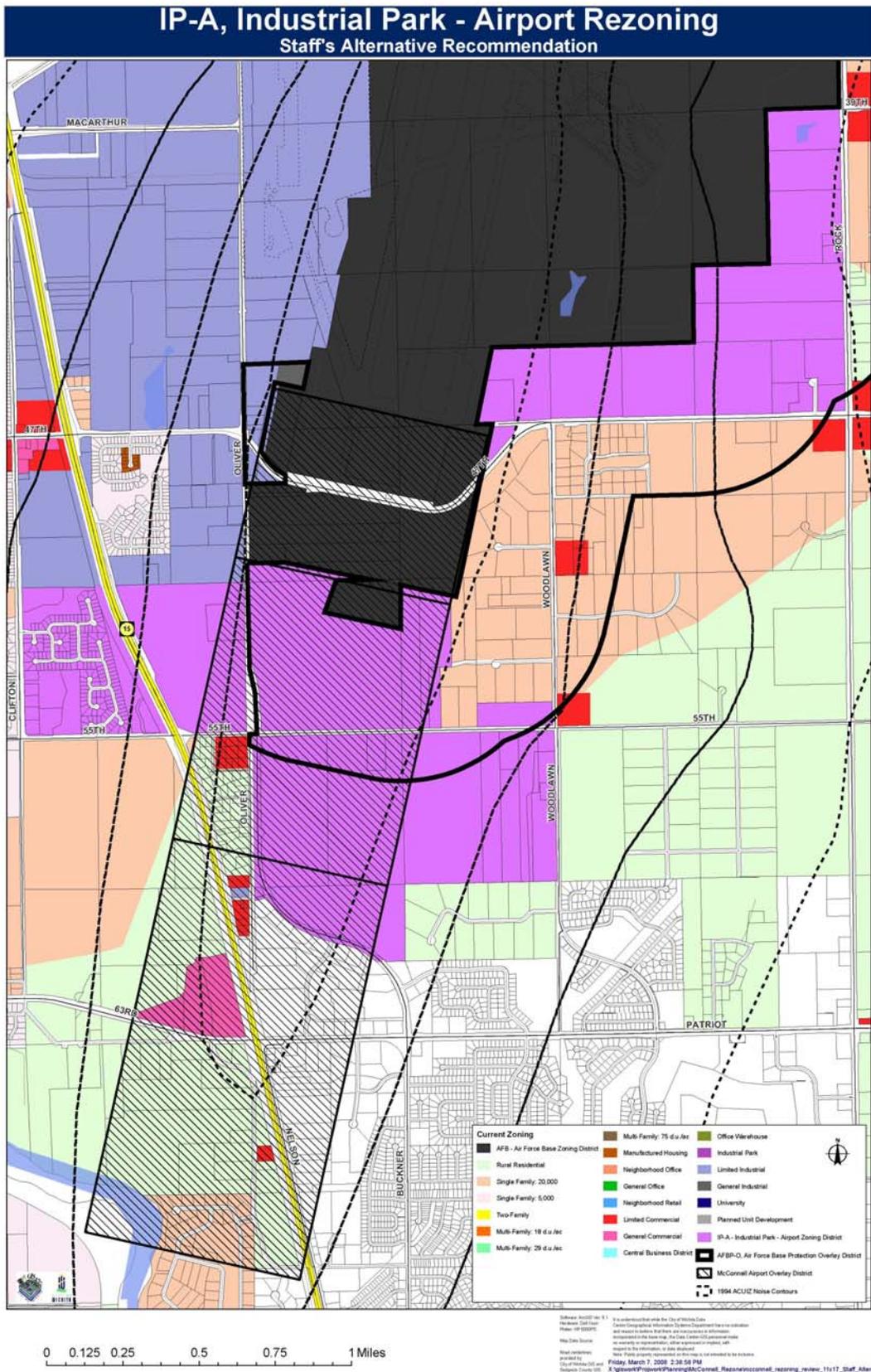
Implementation of the McConnell AFB Joint Land Use Study

At a joint meeting held on August 24, 2005, representatives from the Sedgwick County Board of Commissioners and the City Councils of Derby and Wichita agreed to take action to form an Implementation Coordinating Committee that would help oversee and coordinate efforts to consider the implementation of recommendations contained in the *McConnell Air Force Base Joint Land Use Study, May 2005*. Resulting from a joint, cooperative military and community planning initiative, this Study contains recommendations to balance future operations at McConnell with the growth needs of Derby, Wichita and Sedgwick County.

The Study contains eight recommendations associated with the existing McConnell mission, plus one short-term and six long-term recommendations associated with the potential future McConnell missions. In evaluating each of the Study’s recommendations, the Committee determined that there were several implementation options associated with each recommendation. Following extensive discussion, the Committee reached a general consensus on the following recommended implementation strategies:

- Revise the existing A-O, Airport Overlay District to bring the list of allowable uses into closer conformity with uses recommended in the Federal Land Use Compatibility Guidelines.
- Adopt a new AFB, Air Force Base District to replace the existing SF-20 Residential Zoning classification of the McConnell Air Force Base.
- Adopt a new AFBP-O, Air Force Base Protection Overlay District to restrict the height of any new buildings to a maximum of 25 feet within approximately one-half mile of McConnell.
- Adopt a new IP-A, Industrial Park – Airport District, to replace existing zoning on many agricultural, vacant and undeveloped parcels generally within the 2030 urban growth areas surrounding McConnell. This new zoning district would prevent the creation of new residential development in sensitive areas, while allowing future industrial uses that are consistent with uses recommended in the Federal Land Use Compatibility Guidelines.
- Promote the extension of municipal services to support compatible uses.
- Create a voluntary real estate disclosure related to potential noise from McConnell operations on private property.
- The Wichita Area Builders Association, working with local municipal building officials, would develop noise mitigation standards for any new residential building construction in the area.

The recommendations of the Committee were accepted by the governing bodies in November 2006. Nearly all of the recommended strategies have been implemented, with implementation of the noise mitigation standards anticipated by the end of 2008.



Zoning Area of Influence

The City of Derby's Zoning Area of Influence covers the majority of the plan area from K-15 to 127th Street. As of February 2006, Derby's Zoning Area of Influence was under consideration for revision. In particular, the northern boundary was being considered for changes that would shift the boundary southward. Although a revised Zoning Area of Influence Map was adopted on July 17, 2007, no changes were made to Derby's Zoning Area of Influence boundaries. Refer to Map #37.

Zoning Area of Influence—What is it and what's changed?

In 1985, when Sedgwick County adopted countywide zoning, all of the cities of the second and third class except Eastborough, were granted the authority to review and comment on zoning applications located within close proximity to each cities' boundaries. When a zoning or conditional use application is filed within a city's ZAOI, that city's planning commission must be given the opportunity to review and comment on the application. If the city's planning commission recommends denial, then it requires a unanimous vote of approval by the Board of County Commissioners to override the planning commission's recommendation of denial.

The unanimous vote requirement is a significant hurdle to overcome. Since 1987, planning staff is aware of only four cases where the County Commission approved an application after a city has recommended denial.

In February 2006, the Current Plan's Division of the Metropolitan Area Planning Department started an intensive review of Sedgwick County's "zoning area of influence" (ZAOI) boundaries that has culminated in revised ZAOI boundaries for many of the county's cities. The review of ZAOI boundaries was triggered by changes in the city limits of a number of the Sedgwick County's cities. In some cases, a city had annexed beyond its ZAOI boundary, or revisions were needed to align ZAOI boundaries with projected urban service delivery areas.

Changes were not made to the ZAOI boundaries for: Mt. Hope, Andale, Cheney, Garden Plain, Clearwater, Derby, Goddard or Mulvane. Modifications were made to all the other cities' ZAOI boundaries. The modifications were made only after discussions, between planning staff and each of the cities. The Metropolitan Area Planning Commission reviewed staff recommendations, and then made recommendations to the Board of County Commissioners. County Commissioners Unruh and Parks held special meetings with some city representatives to mediate overlapping boundary requests. At the end of these meetings, agreements had been reached for most of the cities, and the commissioners were able to unanimously adopt a revised map on July 11, 2007.

2030 Urban Growth Area According to the Wichita-Sedgwick County Comprehensive Plan

According to the Wichita-Sedgwick County Comprehensive Plan, a portion of the plan area is within the City of Wichita's 2030 Urban Growth Area, from K-15 to just past Greenwich Road. Refer to Map #38.

What is the Wichita and Small Cities 2030 Urban Growth Area?

The 2030 Urban Growth Areas Map within the Wichita-Sedgwick County Comprehensive Plan is intended to show the anticipated growth patterns for the cities of Sedgwick County. It should be used with a sense of flexibility. Municipal growth patterns that do not exactly match this guide but reflect marketplace demands should be given reasonable consideration, if they do not present extraordinary new public service burdens on the community. It is also important to note that the 2030 urban growth area depicted are not prescriptive nor binding in nature. They serve only as a reasonable indication as to where the future efficient extension of public municipal services and corporate limits could occur by the year 2030.

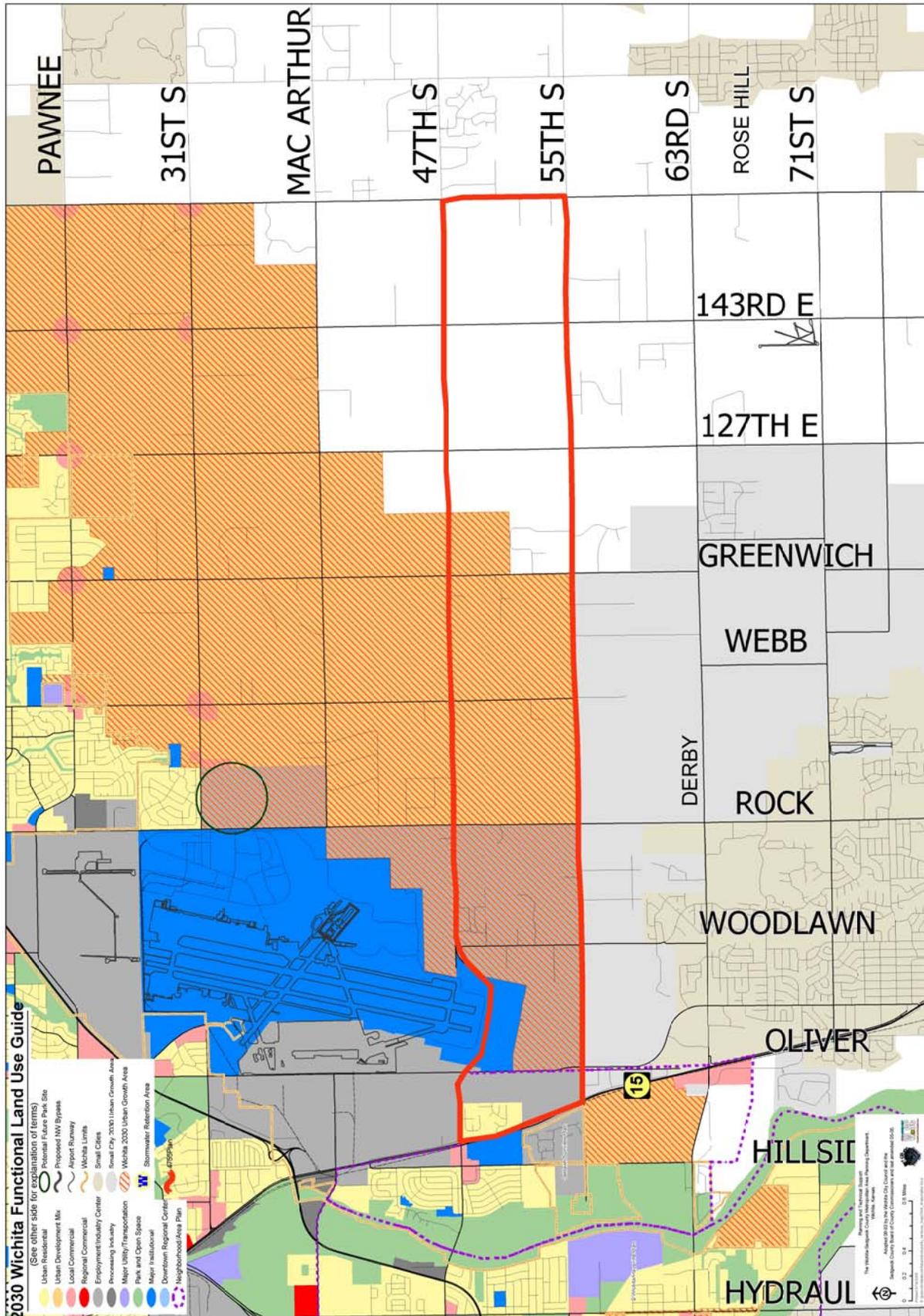
2030 Growth Areas According to the City of Derby's Comprehensive Plan

According to the City of Derby's Comprehensive Plan, the plan area is entirely within the City of Derby's Future Growth Area. The City of Derby 's Comprehensive Plan future land use needs analysis considers future sanitary sewer capacity, population projections, residential density, number of persons per household trends, and land use market trends to identify the amount of land in the planning area which should be considered to accommodate future growth by 2030. The future land use need analysis considered two scenarios to calculate the amount of land which should be used as the planning area for growth by 2030. The two scenarios indicated that a planning growth area of between approximately 6,700 to 7,800 acres should be considered to accommodate growth. The recommended "Near Term Growth Area" to accommodate growth by 2030 is identified on the Growth Area Map #39.

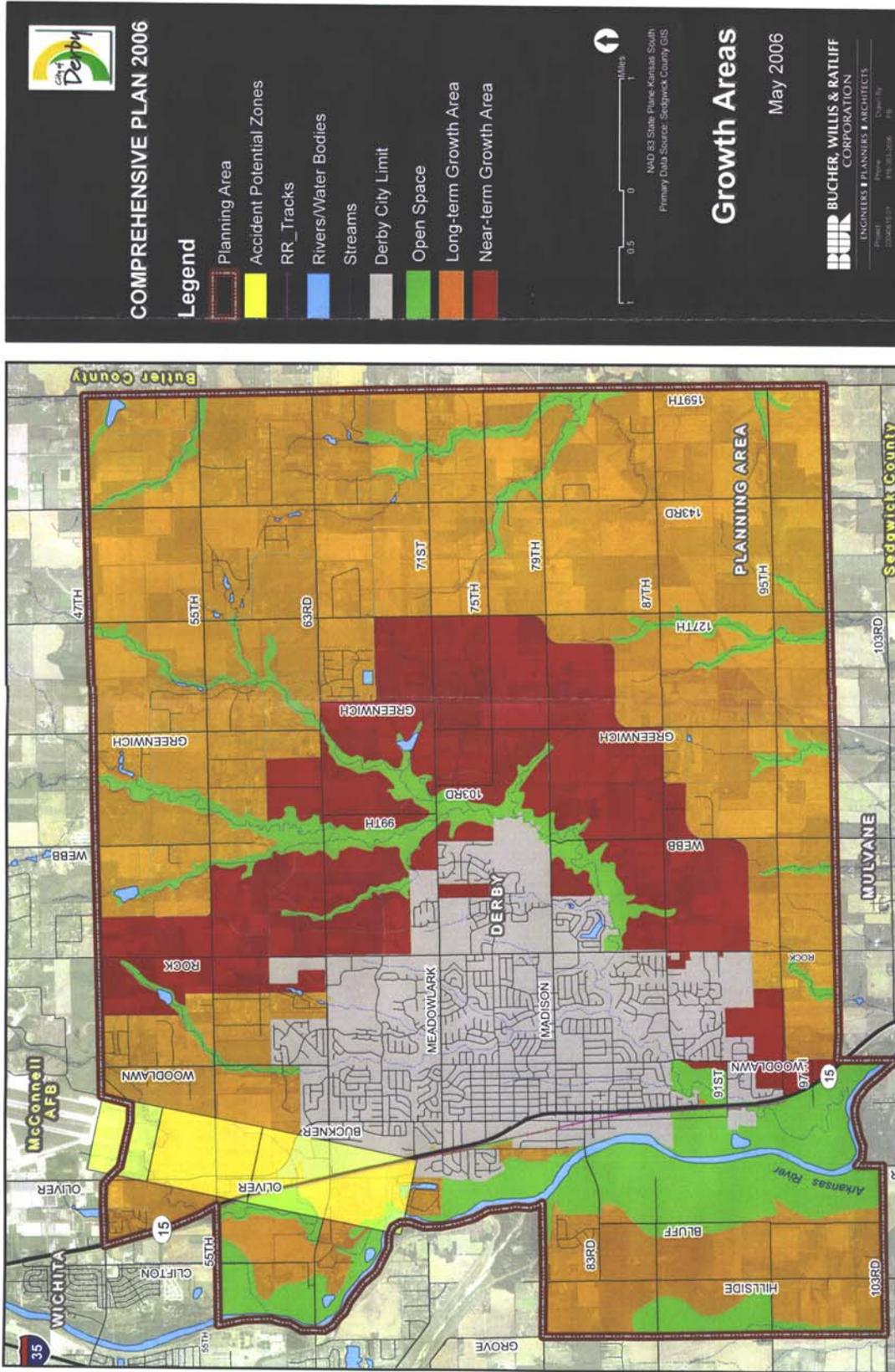
South Area Transportation Study

The plan area falls within the South Area Transportation Study area—a study to address the future mobility and access needs of the southern portion of the Wichita Area Metropolitan Planning Organization (WAMPO) planning region. The study's focus is to develop cost effective transportation improvements to assist economic development and improve quality of life for residents and business owners in the southern planning region. The study analyzed the potential need for a southern bypass, but determined that this would not be a cost effective method, so as an alternative, it evaluated the need for arterial improvements. The South Area Transportation Study is projected to be completed by February 2008. Recommendations within this study will have an affect on how the 47th Street to 55th Street Plan Area develops in the future.

**Map 38: Wichita-Sedgwick County Comprehensive Plan:
2030 Wichita Functional Land Use Guide**



Map 39: City of Derby Comprehensive Plan: 2030 Growth Area Map



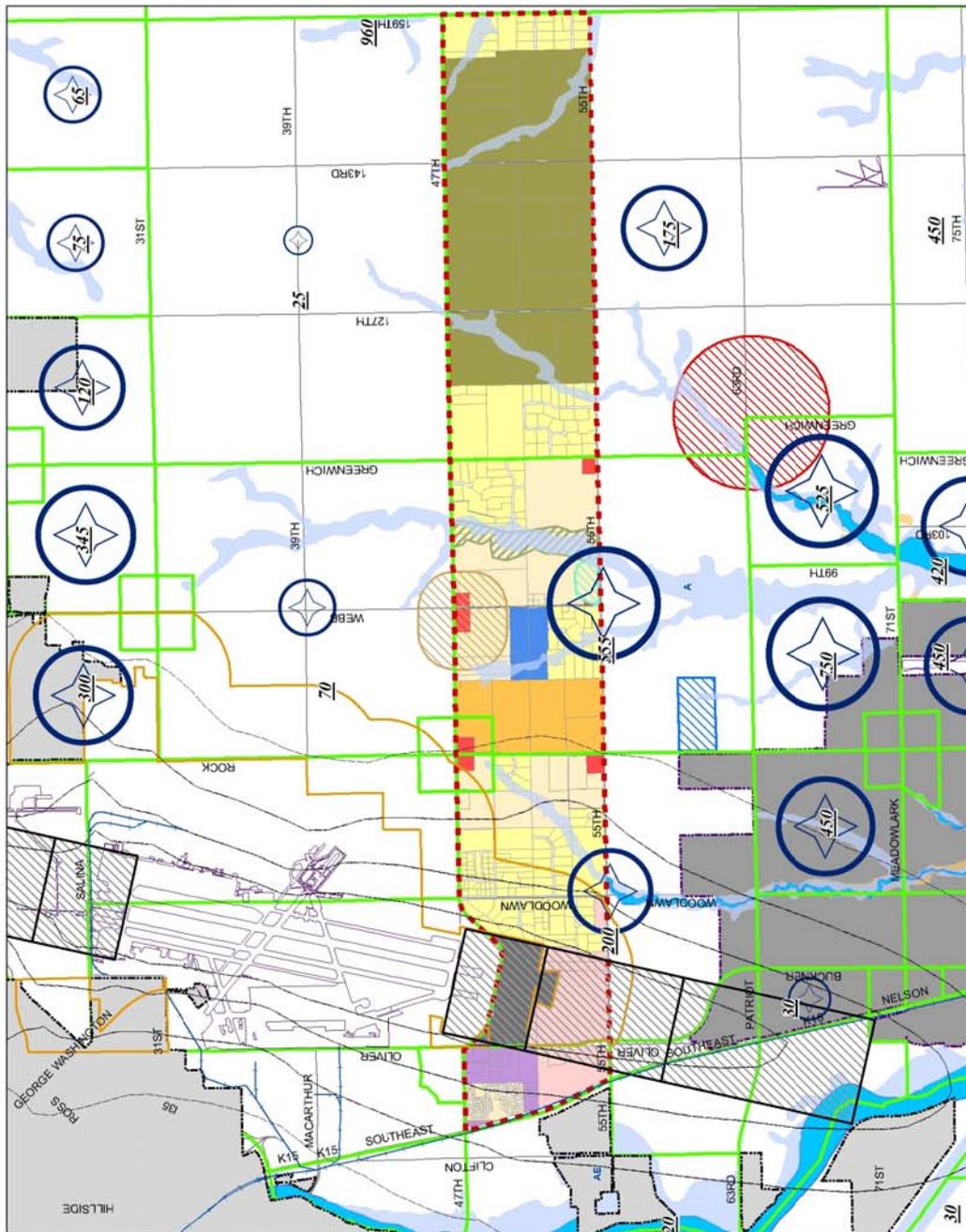
Phase III Future Urban Growth Assumptions

3.1 Housing and Employment Projections

In analyzing the potential population growth and housing development in the 47th Street to 55th Street South Joint Area Plan corridor, MAPD staff reconsidered the numbers developed in 2002-2004 for the 2005 Update to the Wichita-Sedgwick County Comprehensive Plan. Starting with an analysis of building permits in the area south of US-54/Kellogg and east of K-15, it was realized that development in the region was occurring in a more dispersed manner than originally projected. Therefore, some of the growth projected for the area was reallocated to the plan area.

Additionally, a new round of conversations with City of Derby staff indicated a shift in development direction as a result of new sewer projects undertaken in the past couple of years. These shifts were incorporated in their latest Comprehensive Plan. Based upon the new infrastructure plans, many projected dwelling units to the south and east of Derby were reallocated further north along the Rock Road Corridor and along Spring Creek a little further east and north. All in all, approximately 1,000 dwelling units shifted toward or into the plan area. Approximately 500 of the 1,000 units could potentially end up in the plan area by the year 2030.

Employment growth within the plan area is expected to be limited. While more difficult to project, the MAPD staff projects that about 75 retail jobs and less than 200 non-retail jobs may be likely within the plan area.



**45th-55th Street South
Joint Area Plan - 2030
Housing Projections**

- Legend**
- 47th - 55th Joint Area Boundary
 - 1994 ACUIZ Contours
 - AEB Protection Overlay District
 - McCormick Airport Overlay District
 - Airports (Air Base)
 - Traffic Analysis Zones (TAZs)
 - Derby USD 260 Property
 - Buffer of Proposed Firestation
 - Future Wichita Pump Station
 - Buffer of Proposed Wastewater Substation
- Future Land Use**
- Air Force Base
 - Industrial Park - Airport
 - Limited Industrial
 - Local Commercial
 - Institutional
 - Mixed Use Residential
 - Low Density Suburban
 - Rural Suburban
 - Potential Future Park & Open Space
 - Agricultural
- Flood Zones**
- 0.2% Annual Chance Flood Hazard
 - 1.0% Annual Chance Flood Hazard
 - X PROTECTED BY LEVEE
 - Flood Way
- 2030 New Dwelling Units**

It is understood that while the City of Wichita Data Center Geographical Information Systems Department has no indication and reason to believe that there are any errors in the data used to create this map, the Data Center GIS personnel make no warranty of representation, either expressed or implied, with respect to the accuracy, completeness, or data displayed on this map. Neither the City of Wichita nor the Data Center Geographical Information Systems Department shall be liable for any damages or losses, including those caused by reliance on this map, or for any errors or omissions in this map, or for any damages or losses, including those caused by reliance on this map, or for any errors or omissions in this map.

Phase IV—Future Municipal/Community Servicing Concept

4.1 Transportation-External/Internal

The South Area Transportation Study identifies and recommends improvements for four north/south arterials including 143rd Street, Greenwich Road, Webb Road, and Rock Road. The Study also recommends that K-15 be improved from 47th Street to 71st Street. Refer to Map #42.

4.2 Water and Sanitary Sewer Service

Future Proposed Improvement Projects As Identified by Wichita Water Utilities

The City of Wichita's Water Utilities CIP identifies future water main or distribution projects in the plan area, as well as, future sewer mains and a proposed pump station with a force main going to a gravity sewer main.. The sewage collected in these sanitary sewer mains flow to the existing main Sanitary Sewage Treatment Plant at 2305 E. 57th Street South. These water and sewer infrastructure projects are necessary for development to take place in the plan area and construction is projected for the year 2025. Smaller water and sewer pipes would "branch" off the larger lines to serve the future growth areas. Refer to Map #43 & 44.

4.3 Drainage Improvements Scheduled for 2008

According to the Sedgwick County Capital Improvement Program 2008-2012, the plan area does not have any drainage improvements scheduled directly within the plan boundaries, but two drainage improvement projects are scheduled for 2008 just outside the plan area.

Clifton Channel Improvements—(Project Cost = \$1,895,000)

CIP Project D20—Construction of a channel (between Clifton and K-15 from a point just south of 47th Street to a point a quarter of a mile south of 55th Street) to convey drainage from the bridge under K-15 to the existing channel west of Clifton. Improvements will improve the overall drainage in the area, convey runoff from Boeing, and minimize flooding of adjacent properties and prevent flooding of Clifton. Overall this project should significantly reduce the drainage problems #30, #32, and #33.

Channel Realignment and Improvement at 55th Street South and Oliver—(Project Cost = \$410,000)

These improvements will reduce drainage problems (as identified as problem #31)—minimizing flooding of adjacent properties.

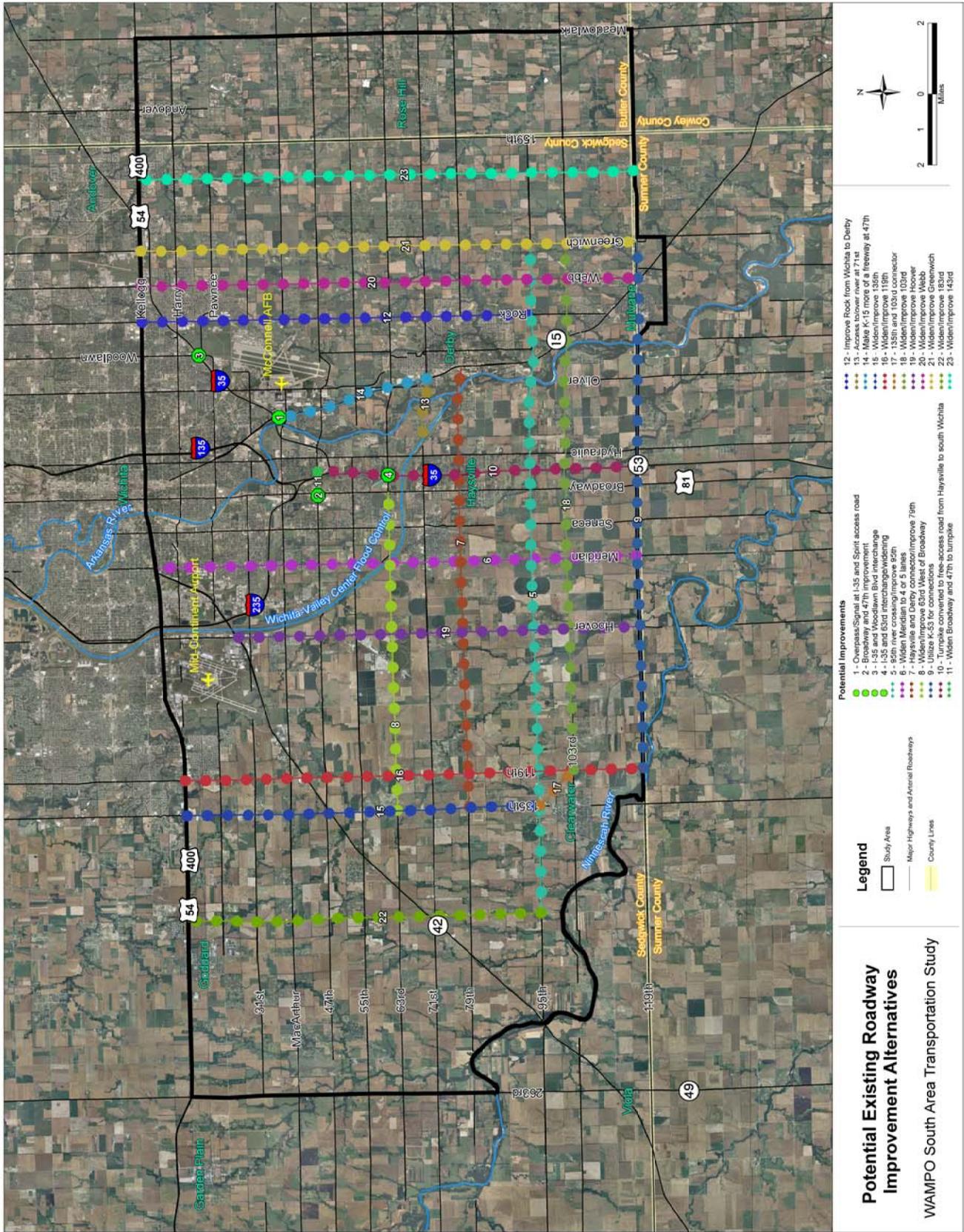
4.4 Electrical

Westar identified a potential site for a future substation in the general vicinity of Webb Road and 47th Street. Please refer to Map #45.

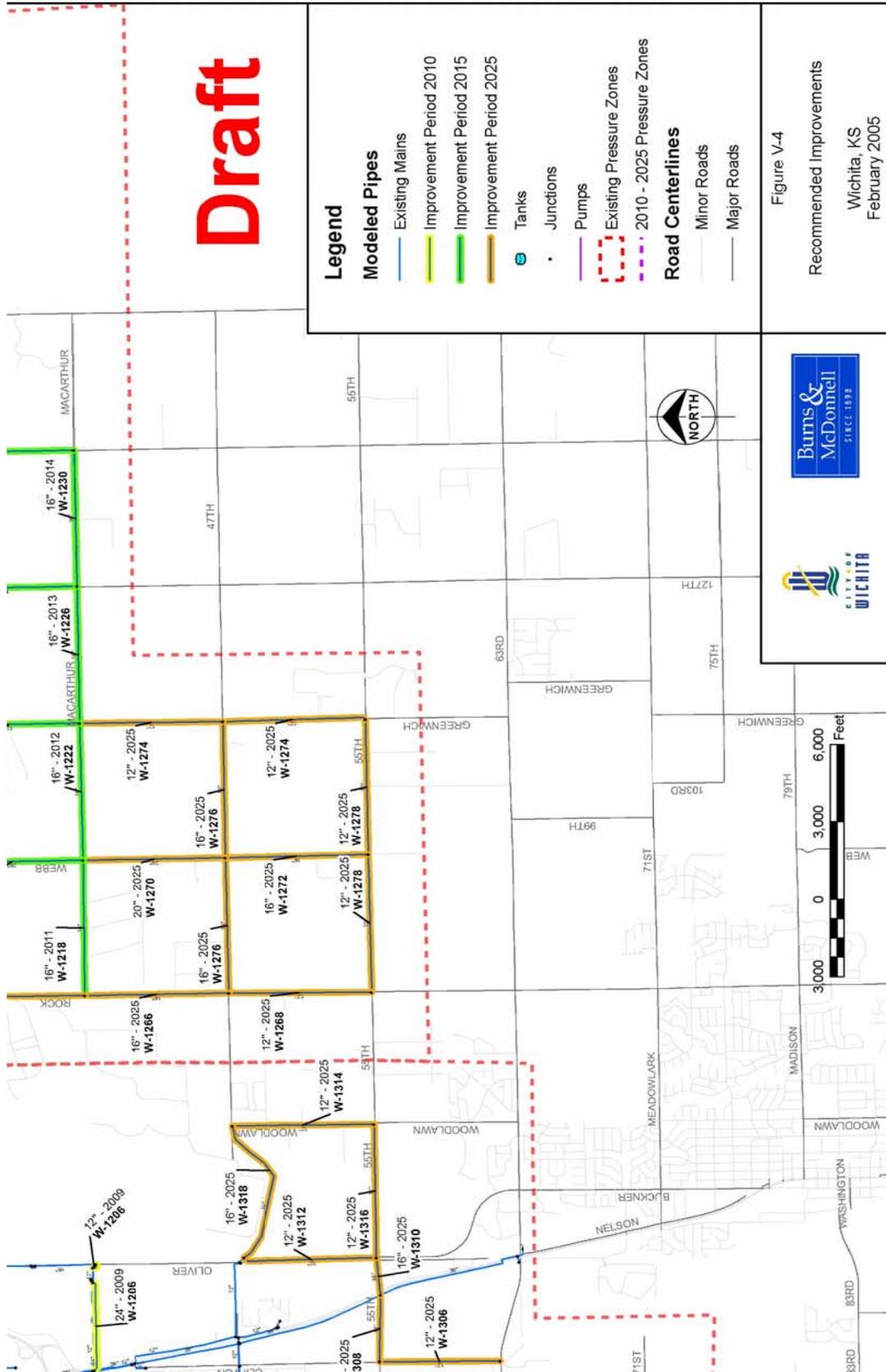
4.5 Law Enforcement

Potential Future Expansion

The City of Derby's Comprehensive Plan recognizes that as the city population continues to grow and its geographic area continues to expand outward, particularly in the Spring Creek drainage basin, the long-term service capabilities of personnel and facilities must be addressed.



Wichita Water Master Plan Future Proposed Water Improvements



Draft

Legend

Modeled Pipes

- Existing Mains
- Improvement Period 2010
- Improvement Period 2015
- Improvement Period 2025

Tanks

Junctions

Pumps

Existing Pressure Zones

2010 - 2025 Pressure Zones

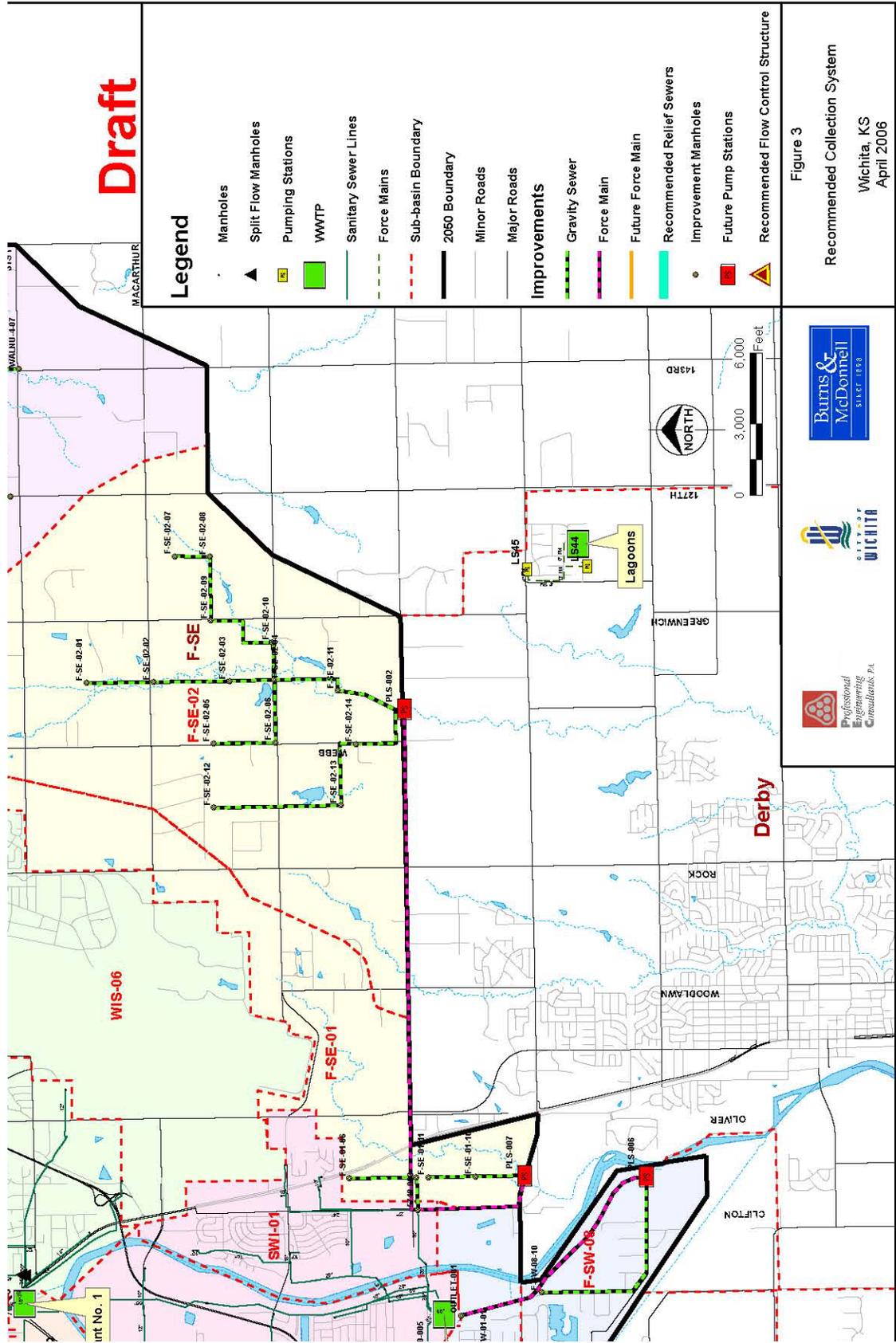
Road Centerlines

- Minor Roads
- Major Roads

Figure V-4
Recommended Improvements
Wichita, KS
February 2005



Wichita Sanitary Sewer Master Plan Future Proposed Sewer Improvements



Phase IV—Future Municipal/Community Servicing Concept

Figure 3
Recommended Collection System
Wichita, KS
April 2006



4.6 Fire Protection

Future Changes in Fire Service

Sedgwick County's Capital Improvement Program allocated \$1,921,086 for the relocation of Station #36 about two miles to the east, in the vicinity of 63rd and Greenwich Road, scheduled for March or April of 2009. The relocation would improve the response times, changing the range from three (3) to eleven (11) minutes. The City of Wichita and Sedgwick County have a first-responder agreement that would also assist with the western portion of the plan area.

In addition, the City of Derby's Comprehensive Plan identifies the need to address the long-term fire protection needs in the Spring Creek growth area and identifies general locations for future fire stations to serve the 2030 future growth area based on the current service response radius of 1.5 miles. Refer to the Derby Comprehensive Plan, "Public Facilities Map."

4.7 Library Service

In October 2007, voters in the City of Derby elected to build a new public library to meet the needs of the growing population for the next 25 years. The facility is being designed, and construction is planned for 2008-2009. The library site is at the corner of Walnut Grove and Sumac streets, just east of Tanglewood Elementary School and just one block north of City Hall in Derby. For nearly 50 years, the City has provided a public library, which has grown in popularity among residents and non-residents. In 2007, patrons were estimated to include 2/3 Derby residents and 1/3 non-residents.

4.8 Schools

The Derby USD 260 purchased a large property east of Rock Road, between 63rd Street and 55th Street as the site of a potential future high school. Due to the recent drop in enrollment numbers, it is not anticipated that a new school would be constructed for at least 10 years or longer. In addition, Derby USD 260 purchased another property northeast of Woodlawn and 63rd Street South in the Stone Creek Addition for a future elementary school. These future school sites will eventually have a significant impact on the development patterns and service needs within the plan area.

4.9 Park/Open Space

The Wichita Parks, Recreation, and Open Space Plan (PROS Plan) is being developed to help guide the development, provision, maintenance, and funding of park, recreation and open space resources for the City of Wichita. The Wichita-Sedgwick County Metropolitan Area Planning Department, Wichita Park and Recreation Department, and Wichita Board of Park Commissioners have been working diligently since December 2006 to identify community PROS needs; to develop a long range vision, and to develop strategies for implementation. It is anticipated that this Plan will be adopted by the Wichita City Council as an element of the Wichita-Sedgwick County Comprehensive Plan in late 2008.

The PROS process has included coordination with other area communities, including Derby, to ensure that Wichita is providing public PROS resources as efficiently as possible. The Wichita Park Board and City staff are working with Visioneering Wichita to develop a process that communities within the region can utilize to share and coordinate their PROS planning efforts.

Currently, the provision of civic PROS resources are guided by the 1996 Wichita Parks and Pathways Plan, and the 1999 Wichita Park and Recreation Facilities Study. The Wichita PROS Plan will combine these efforts into one document that will address these important quality of life, infrastructure, health, and economic issues.

Phase V—Future Land Use Concept

5.1 Future Land Use Categories

Depicted on Map #45 is the Future Land Use Concept proposed for the plan area. As a result of several recommendations from the McConnell Air Force Base Joint Land Use Study, a series of overlay districts and special zoning classifications have recently been created and approved that will affect the future land use pattern of the western portion of the 47th-55th Street South Joint Area Plan Area. Below is a description of each zoning category:

Overlay Districts & Special Zoning Classifications Influencing Future Land Use

Air Force Base District- The purpose of the AFB district is to accommodate the use of lands owned by the government of the United States for the purpose of operating an air force base. The AFB district is generally compatible with the "Major Institutional" designation of the Wichita-Sedgwick County Comprehensive Plan. It is intended only for application on air force base property.

AFB Protection Overlay District - The AFBP-O air force base protection overlay district is intended to provide protection to McConnell Air Force Base through site development regulations for properties adjacent to the air force base that limit the risk of terrorist activities from threatening the air force base and its personnel.

McConnell Airport Overlay District - The intent and purpose of the A-O airport overlay district regulations is to specify land use controls in addition to those in underlying zoning district that will ensure a compatible relationship between air force base operations and other land uses in the vicinity. The A-O district standards will protect both the public and the airport by reducing to a minimum, land uses that concentrate large numbers of persons underneath runway takeoff and approach paths, where aircraft accidents are most likely to occur and noise levels are inappropriate for urban-density residential and high intensity nonresidential uses. The area protected falls into both hazard zones and accident potential zones. In the event of conflict between the A-O standards and any other regulation applicable to the same property, the more restrictive regulation or standard shall govern and prevail.

Industrial Park- Airport - The purpose of the IP-A industrial park - airport district is to accommodate research and development, industrial and manufacturing uses that can meet high development and performance standards and are compatible with the operation of airport and related facilities. The IP-A district is generally compatible with the "Industrial" designation of the Wichita-Sedgwick County *Comprehensive Plan*. It is intended for application primarily within the City of Wichita, although it may be appropriate for application in areas of unincorporated Sedgwick County that have been designated as "growth areas."

Limited Industrial - The purpose of LI limited industrial district is to accommodate moderate intensity manufacturing, industrial, commercial and complementary land uses. The LI district is generally compatible with the "industrial" designation of the Wichita-Sedgwick County Comprehensive Plan. It is intended for application primarily within the City of Wichita, although it may be appropriate for application in areas of unincorporated Sedgwick County that have been designed as "urban service areas."

Other Potential Future Land Uses

The future land use concept for all other areas is based on current development trends, population projections, the anticipated ability to provide services, existing uses and other anticipated uses as indicated in the Wichita-Sedgwick County Comprehensive Plan and the City of Derby Comprehensive Plan. Future development proposals for properties impacted by the operations of McConnell Air Force Base should be reviewed in light of the recommendations of the Joint Land Use Study, and additional overlays or development plans may be necessary to ensure compatibility with Air Force operations. Below is a description of each future land use category:

Local Commercial - This category encompasses areas that contain concentrations of predominately commercial, office and personal service uses that do not have a significant regional market draw. The range of uses includes medical or insurance offices, auto repair and service stations, grocery stores, florist shops, restaurants and personal service facilities. On a limited presence basis, these areas may also include mini-storage warehousing and small scale, light manufacturing uses.

Institutional - This category includes all public, semi-public, and institutional uses of significant size and scale such as schools, churches, post offices, hospitals, libraries, cemeteries governmental uses, and religious institutions.

Mixed Use Residential - This category promotes a variety of medium to high-density residential land uses including single-family, two-family, townhouse, condominium, and multifamily apartment dwellings, which may be intermixed throughout the neighborhood. In addition, small offices (i.e. medical, professional services, financial services, etc.) and limited retail stores (i.e. bakery, dry cleaners, small neighborhood eating establishments with limited seating, etc.) are permitted in this category only through planned zoning and by establishing site design, architectural, and land use controls with development approval. Such commercial uses are intended to provide services only to the residents of the surrounding area and placed in locations with a design character that blends into the neighborhood, rather than backing up to or screened from residential. Parking areas for retail uses are small due to limited parking needs. Uses not compatible for such an area include automotive related businesses, drive-through businesses, conventional strip pad sites, high traffic generating uses or businesses requiring large parking lots, uses oriented toward a regional customer service area beyond nearby neighborhoods, and other uses deemed not compatible in close proximity to residential uses.

All areas of a Mixed-Use Residential area must be designed in a manner to promote pedestrian activity through a system of interconnected streets and varied streetscapes that also provide safe and efficient movement of vehicular traffic. Residential densities may range up to 6-10 units per acre, but will tend to vary throughout the neighborhood.

Low Density Suburban Residential - (generally up to 3 units per acre) This category is to accommodate residential development for areas surrounded by or near Rural/Suburban Acreages in which a full range of municipal water, sewers, and other services are not currently available, but may be extended in the long-term future. Development in this category should be limited until municipal services are provided, and should be subject to a "transition" policy addressing development layout, lot sizes, density, and other elements to minimize growth conflicts when located adjacent to suburban acreages with different levels of compactness.

Rural Suburban - (generally 1 unit per 5 acres or more, unless developed as a cluster design subdivision): This category reserves land area developed with rural single-family dwellings on large lots, typically served by on-site private sanitary sewer/septic systems, commonly called acreages. Such areas are typically already developed with residential dwellings on 1-to-5 acre or larger parcels, and the full range of municipal services are neither available nor planned. A city is not expected to extend municipal water, sewer, or other services to areas within this category.

Park, Recreation, and Open Space - Areas of predominately active and passive parks, open space, recreation, environmentally sensitive areas, or any other lands reserved for permanent open space purposes. Land identified as preferred or acceptable areas for public parks are distinguished from other open space. This category includes woodlands, land within floodplains, and golf courses. Floodplains in future growth area should be preserved for open space, parks, wildlife habitats, natural preserves, golf courses, hike and bike trails, and storm water drainage.

Agricultural - (generally 1 or fewer units per 20 acres): Land area principally in use for agricultural production and may be used for farming, crops, pasture, agribusiness ventures such as growing and marketing of products, and a limited number of rural residences. Such area is predominately located in the unincorporated area and subject to Wichita-Sedgwick County Zoning Regulations. This category also serves as a holding zone to preserve land from premature development that would negatively affect the area while preserving the agricultural uses in the immediate area.

5.2 Community Identity Elements

Both the Wichita-Sedgwick County Comprehensive Plan and the Derby Comprehensive Plan identify sites or general locations for creating community identifier features or elements. Within the 47th-55th Street South Joint Plan Area, the Wichita-Sedgwick County Comprehensive Plan identifies two secondary gateways to be located on the north side of 55th Street along Rock Road and along K-15. The City of Derby's Comprehensive Plan does not identify gateways within the plan area, but identifies two gateways directly south, approximately one mile, from the Wichita-Sedgwick County proposed gateways. Derby identifies a total of five gateway locations throughout the City—two of which are north of 63rd Street at K-15 and at Rock Road.

The 47th-55th Street South Joint Area Plan Steering Committee and Technical Advisory Committee discussed both the Wichita and Derby proposed gateway locations and reaffirmed that these sites are the preferred locations. No changes to either comprehensive plan are necessary with regard to gateways. It is recommended that gateways are only located in areas that are within that jurisdiction's city limits. The gateways within or near the plan area will most likely not be constructed until the Wichita and Derby jurisdictional boundaries converge. Refer to Map #46.

Background on Visual Form within the Wichita-Sedgwick County Comprehensive Plan

The 1993 Wichita-Sedgwick County Comprehensive Plan included a Visual Form Map in an effort to improve the quality of the physical built environment in Wichita. It focused upon vistas, corridors, landmarks and other community amenities for upgrading the public environment, improving standards for public and private development, and creating greater community awareness of the importance of its visual image.

In August 2005, the City of Wichita's Design Council requested the assistance of MAPD staff in updating the Visual Form Map. Following several meetings, The *Priority Enhancement Areas for Wichita Public Infrastructure Projects Map* was developed to replace the 1993 Visual Form Map. It is intended to guide efforts to improve community perception and increase the sense of quality of life in Wichita through emphasis of the visual character of public facilities and open spaces. Its purpose is to help the City with prioritizing City of Wichita public works projects along specified corridors, at gateways, and at other selected locations for aesthetic improvements including landscaping, public art, and other visual enhancements to public facilities and right-of-way. The *Priority Enhancement Areas for Wichita Public Infrastructure Projects Map* was adopted as an amendment to *The Wichita-Sedgwick County Comprehensive Plan* on July 12, 2006.

Background on Gateways within the City of Derby's Comprehensive Plan

The City of Derby 2006 Comprehensive Plan's Future Land Use Plan states, "Gateway areas are identified for areas key to promoting a positive image to the residents and visitors as they enter and exit the City. Special consideration should be given to development projects in such areas. Also, gateways should include community investments in special entry features, sculptures, statues, signage, landscaping, streetscaping, or other improvements that will improve the overall image of the City." Chapter 6, Land Planning Principles, of Derby's Comprehensive Plan also identifies strategies for implementation of gateways as follows:

Primary gateways should identify the City of Derby as a whole and should be located at the entrances to the City along major roadways such as at the north and south ends of K-15 and the north and south ends of Rock Road. The major features of primary gateways should include:

- A prominent feature such as large-scale public art, statuary, fountains, gardens or park-like settings;
- Signage to clearly identify entrance into Derby. Brick, stone or other high-quality materials and the City logo should be incorporated into the signage to further establish the gateway; and
- Landscaping to accent the surrounding corridor and prominent features. A combination of street trees, ornamental trees, shrubbery, ground covers and ornamental plantings should be used to accent and coordinate the design.

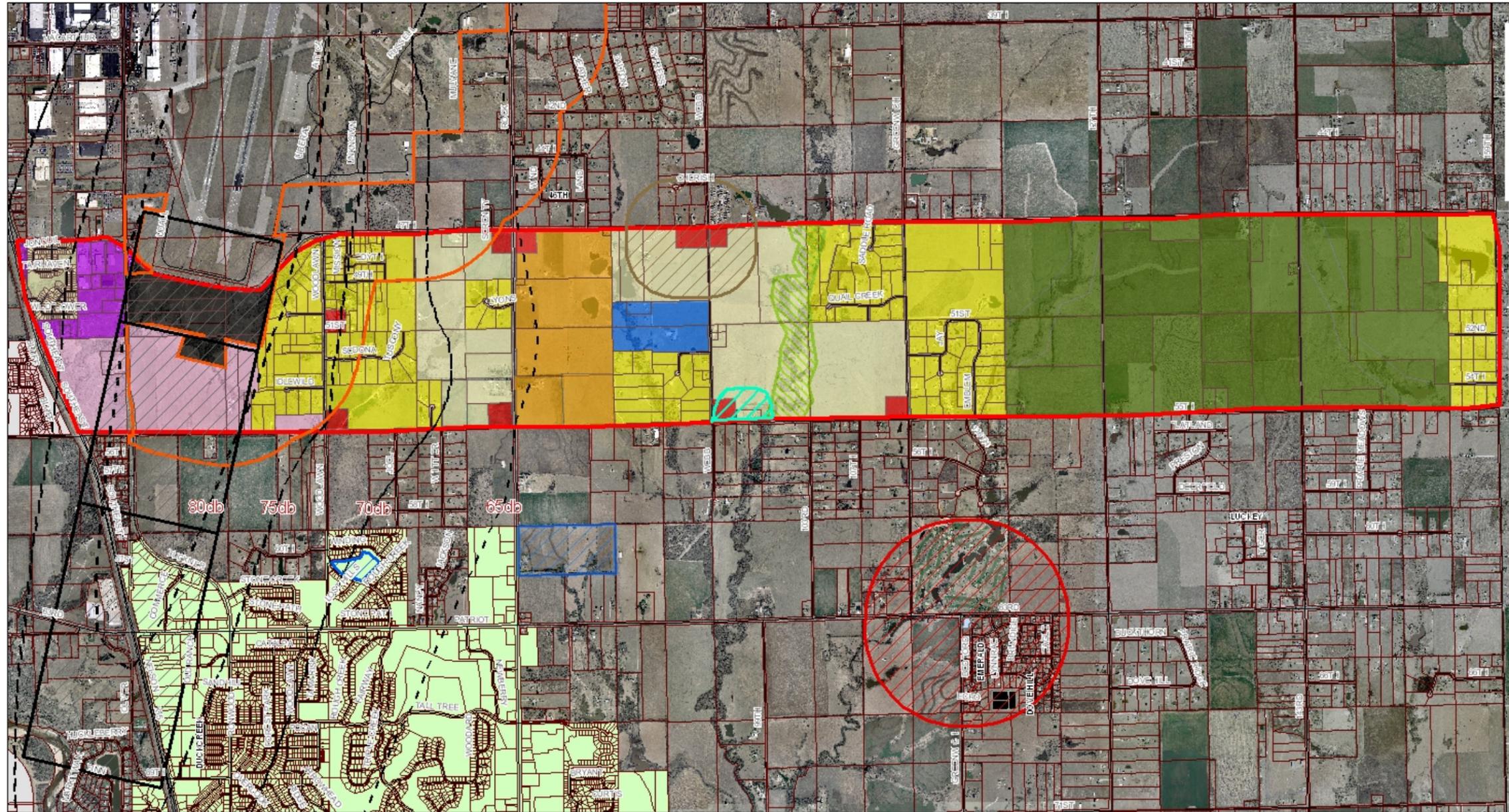
Secondary gateways should identify the City of Derby, but on a smaller, pedestrian scale than the primary gateways. These gateways should be located at secondary entrances into the City, such as along the Arkansas River at Market Street.

5.3 Phasing of Development

The 47th-55th Street South Joint Area Plan Steering Committee and Technical Advisory Committee identified three areas that are most likely to develop based on existing development patterns, zoning classifications, and/or proposed future servicing areas. The "Phases of Development" include the Rock Road Corridor Phase, the Industrial Phase, and the Spring Creek Phase. These phases of development are general areas where development is anticipated to occur with no specific timeframe in mind. Refer to Map #46.

- The Rock Road Corridor Phase is identified based on existing development patterns along Rock Road, which is anticipated to continue south toward Derby.
- The Industrial Phase is identified based upon the implementation of recommendations contained in the McConnell AFB Joint Land Use Study, including some new zoning classification as previously discussed in section 5.1.
- The Spring Creek Phase is identified based on the potential future sewer pump station that has been identified to potentially be constructed by 2030—within this Plan's horizon.

47th-55th Street South Joint Area Plan Future Land Use Concept



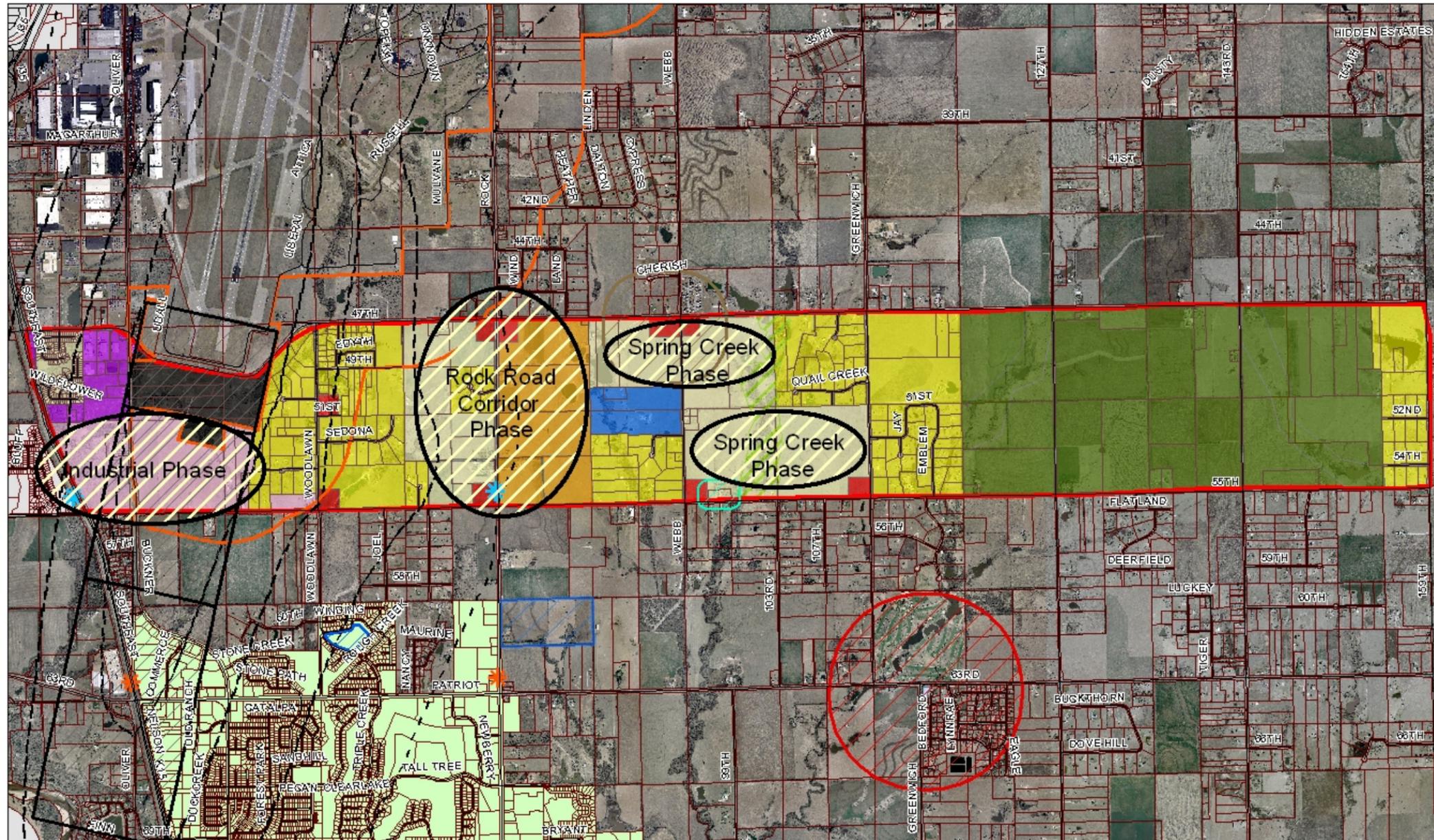
- | | | | |
|---|---------------------------|------------------------------------|------------------------------------|
| Plan Area | Air Force Base | Mixed Use Residential | 1994 ACUIZ |
| Proposed S.C. Fire Station - General Vicinity | Industrial Park - Airport | Low Density Suburban | AFB Protection Overlay District |
| Proposed Westar Site - General Vicinity | Limited Industrial | Rural Suburban | McConnell Airport Overlay District |
| Future Wichita Sewer Pump Station | Local Commercial | Potential Future Park & Open Space | DERBY |
| Derby USD 260 Future School Site | Institutional | Agricultural | WICHITA |



Software: ArcGIS
 Map Data Sources:
 City of Wichita
 Sedgewick County
 PR: paid: 12/31/07

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 Note: Public property represented on this map is not intended to be inclusive.

47th-55th Street South Joint Area Plan Future Land Use Concept - Phasing of Development



- | | | | | |
|--|---------------------------|------------------------------------|------------------------------------|-------------------------------------|
| Plan Area | Air Force Base | Mixed Use Residential | 1994 ACUIZ | Derby's Proposed Gateway Location |
| Proposed S.C. Fire Station - General Vicinity | Industrial Park - Airport | Low Density Suburban | AFB Protection Overlay District | Wichita's Proposed Gateway Location |
| Proposed Westar Site - General Vicinity | Limited Industrial | Rural Suburban | McConnell Airport Overlay District | |
| Future Wichita Sewer Pump Station - General Vicinity | Local Commercial | Potential Future Park & Open Space | DERBY | |
| Derby USD 260 Future School Site | Institutional | Agricultural | WICHITA | |

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Software: ArcGIS
Map Data Sources:
City of Wichita
Sedgwick County
Prepared: 02/2007



Phase VI—Tri-Jurisdictional Task Force

6.1 Purpose of the Tri-Jurisdictional Task Force

The Steering Committee recommends that the Managers of City of Wichita, the City of Derby and Sedgwick County create a Tri-Jurisdictional Task Force to continue efforts to coordinate the future growth and servicing issues that may develop over time. The Task Force would meet once a year, (or more often as needed) and would consist of one person from each jurisdiction that is familiar with these issues. Although this Plan provides land use guidance, there are some unresolved issues that the Committee felt that it was too early to make any further decisions. At this time, it is recommended that the water agreement not be modified between Wichita and Derby, and that annexations continue to occur as the market demands. This annual meeting would allow Wichita, Derby, and Sedgwick County the ability to continue coordinating these efforts and the ability to potentially recommend amendments to the Plan if necessary.